



CONTEXT

Welcome to our Open House!

We're here to listen! This is your opportunity to share your thoughts on the future of Central Lonsdale. We're exploring improvements to the public realm, land use, economic development, transportation, and infrastructure – and your input is essential to the success of this project. Please take your time to review the boards and let us know what you think.

Here's how you can get involved:

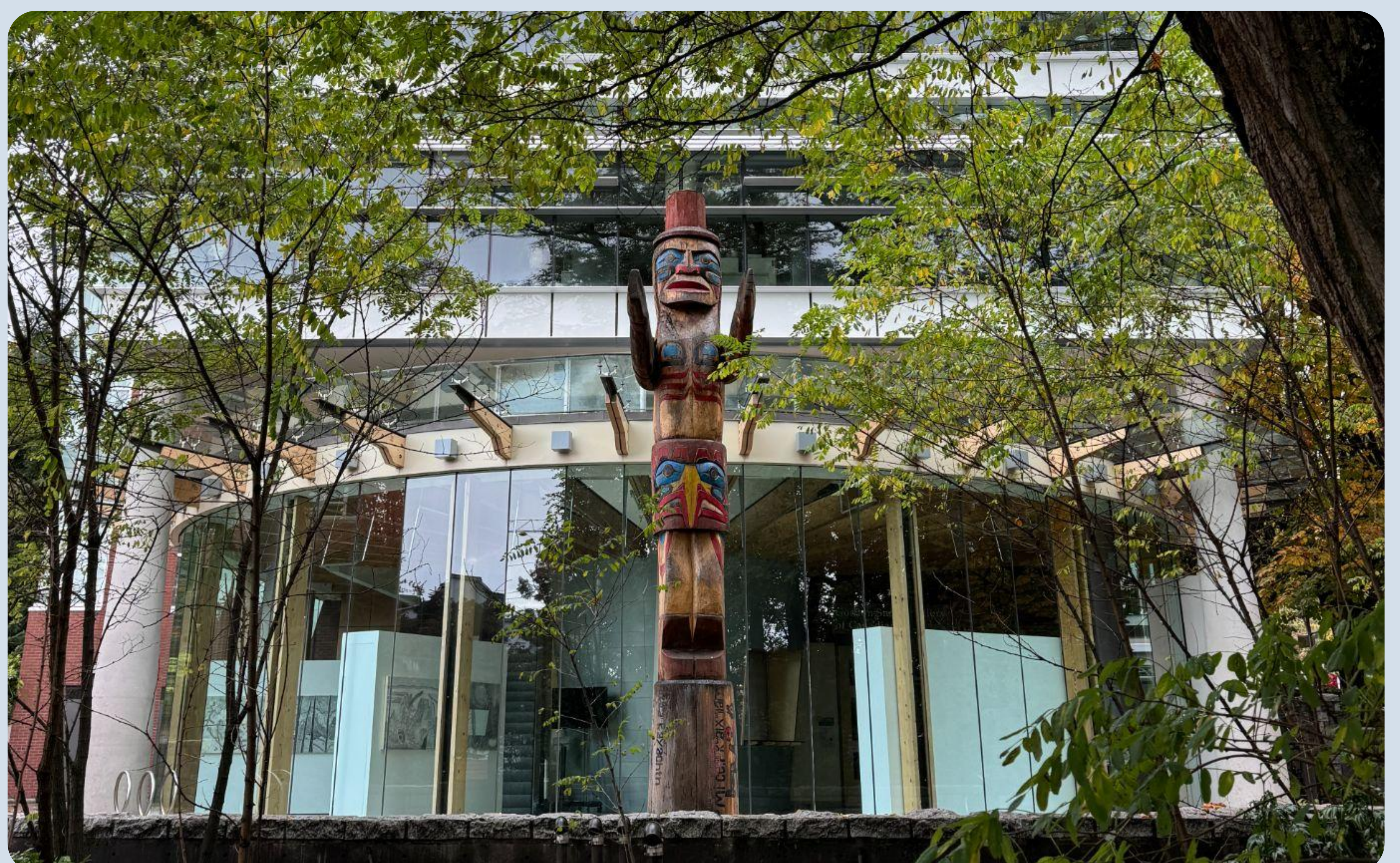
- 1 Explore the boards:** Learn about some of the opportunities for improving Central Lonsdale.
- 2 Share your feedback:** Write your comments on the boards, talk to our team members, or share your ideas.
- 3 Help us shape the future:** Your input will directly influence the vision for Central Lonsdale.



Land Acknowledgement

We respectfully acknowledge that we live and work on the traditional and unceded territories of the Sḵwxwú7mesh (Squamish) and səliłwətał (Tsleil-Waututh) Nations.

The City of North Vancouver is committed to reconciliation with these Nations, who have lived on these lands since time immemorial.





CONTEXT

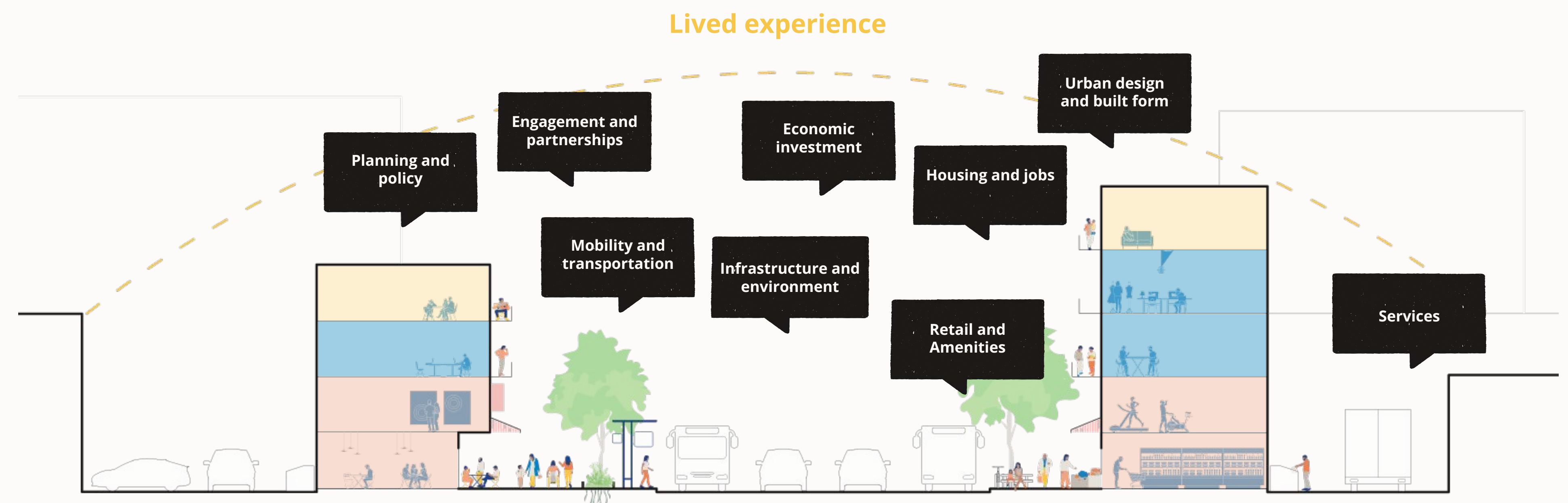
Welcome to our Open House!

Lonsdale Great Street: Why now?

The Lonsdale Great Street project is about more than just a street. It's about creating a thriving and welcoming Central Lonsdale for everyone by strengthening the vital connections between the public realm, the built environment, and the local economy, ensuring these connections benefit both people and nature.

Central Lonsdale, like many growing areas, is evolving, and with this evolution comes the need for our shared spaces to respond. More people are expected to call this place home, and with the need for more housing, the streets, parks, and gathering spaces will need to serve a wider range of activities within the same space.

This project is important now because it will help us proactively shape a Central Lonsdale that will serve an expanding and diverse community, adapt to our changing climate, and keep the local economy strong and vibrant.



Enhancing the lived experience

To create a positive lived experience, we need to foster a place where people feel welcome, comfortable, and connected. To understand how people experience Central Lonsdale, we conducted an online survey, and over 2,600 people responded! They told us they would like to see more comfortable places to sit and relax, diverse dining options, inviting spaces to hang out, public art, more trees, and a less car-centric environment that's great for walking.

Fostering economic prosperity

A thriving economy is essential to the vitality of Central Lonsdale, just as a vibrant public realm enhances the human experience. Central Lonsdale is already experiencing significant economic success, with a diverse and resilient mix of businesses, from unique shops and restaurants to essential health and wellness services. This thriving business environment is a testament to the area's appeal and its ability to attract both customers and investment.

Building a resilient future

Building a resilient future requires an approach that considers not only the needs of the present but also the well-being of future generations. This means creating a strategic framework that can adapt to the challenges of a changing climate and preserving its unique ecological and cultural heritage.

Central Lonsdale has a rich ecological and cultural landscape. Opportunities exist to enhance views of the surrounding natural beauty, celebrate the region's indigenous heritage and cultural diversity, and create a more vibrant public realm that reflects the community's identity.

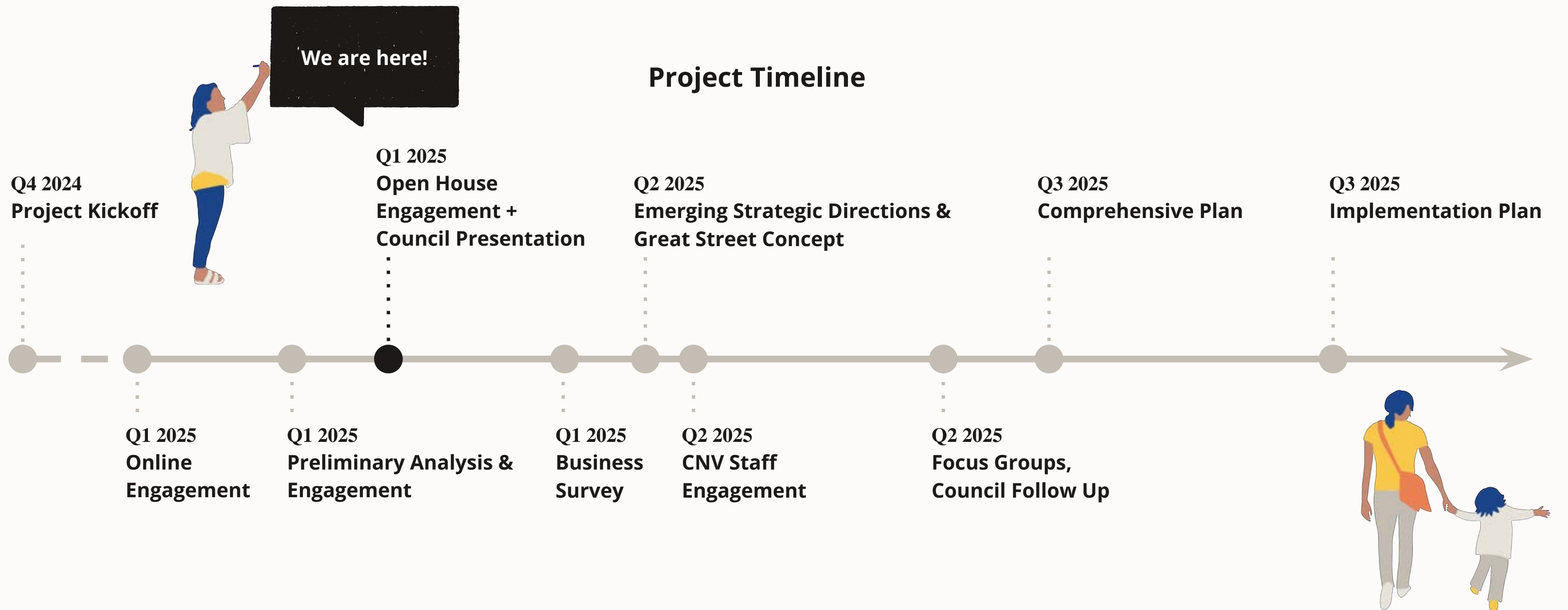
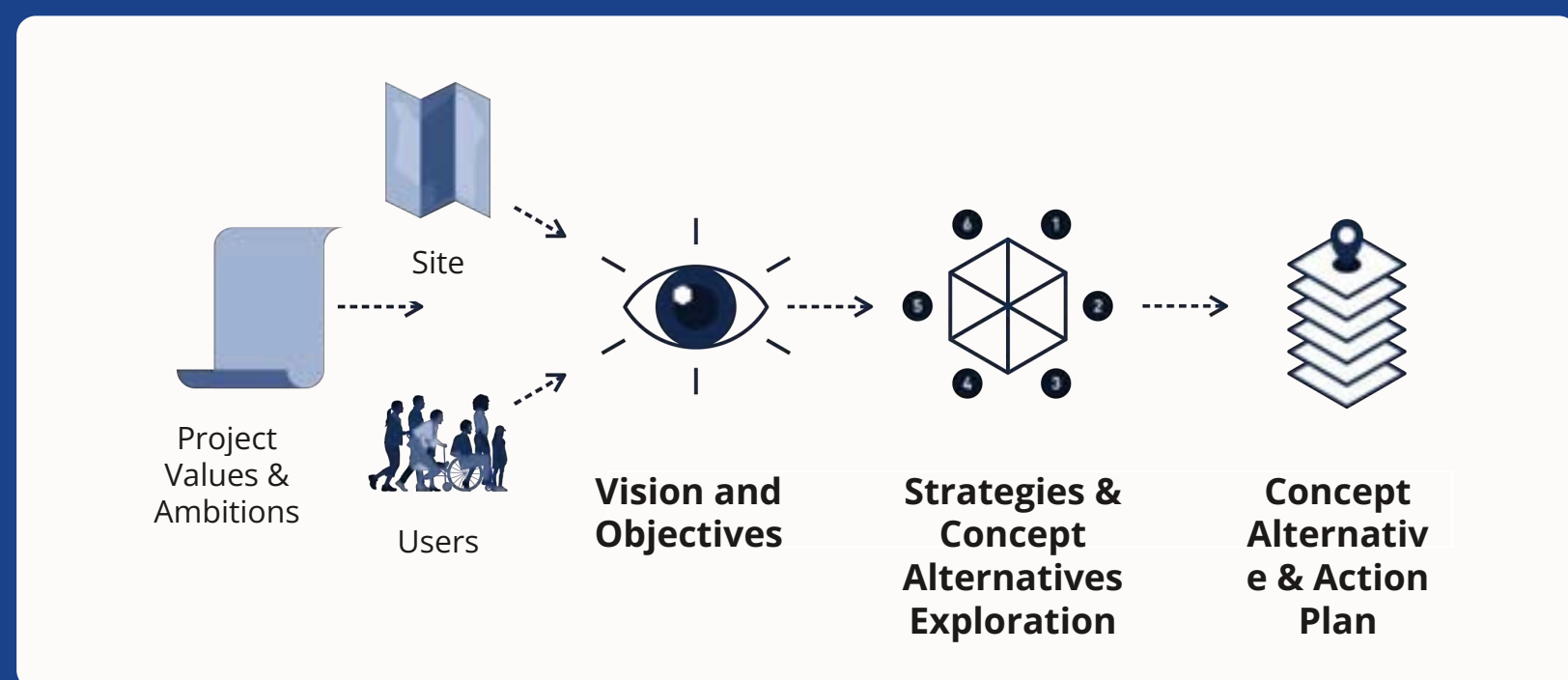
Embracing collaboration

Creating a successful future for Central Lonsdale is a shared responsibility. It requires working together with community members, businesses, and partners in the public, regional, and private sectors to achieve common goals.

Collaboration also means recognizing that not everyone will get everything they want. Transforming Central Lonsdale will involve navigating tradeoffs and finding solutions that balance competing interests.

Project Process

This project for Central Lonsdale offers an opportunity to make sure that change is rooted in the needs and desires of the community at large. Rather than a fixed plan, this project will deliver a strategy and action plan for transformation over time.



We used a robust, multi-method research process to understand Central Lonsdale today

Our research process captured perspectives of residents, workers, property & business owners, and visitors.





PUBLIC REALM

Running Errands & Recreating in Central Lonsdale



Lonsdale Avenue: A Through-Street Today, A Place For People Tomorrow?

Lonsdale Avenue is a vibrant, walkable corridor lined with fine-grain retail and a diverse mix of local businesses. Its human-scaled buildings and intimate atmosphere, though limited to specific street segments, naturally draw pedestrians, making these areas the most active parts of the street.

Aside from these pockets of activity, the street is currently defined by vehicular movement, serving to get people from point A to B, rather than by what creates a pleasant pedestrian experience. The community is enthusiastic about fostering what makes Lonsdale Avenue vibrant today and further establishing it as a distinct neighbourhood destination that invites public life and offers reasons to spend time.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:



What we heard:

On a typical visit to Central Lonsdale:
45% respondents spend 1-2 hours.
25% respondents spend 2-4 hours.



“Even though there has been an attempt to add curbside seating areas, it's not a particularly inviting area to sit and hang out. More trees, more green spaces may help with that.”

— 2025 Lonsdale Great Street Survey Respondent

“This is an opportunity point to make Central Lonsdale fresh and exciting...”

— 2025 Lonsdale Great Street Survey Respondent

Opportunities:

- **Establish a distinct visual identity:** Integrate a unified palette of design elements, from paving and seating to light fixtures and signage, to reinforce Lonsdale Avenue's unique character and create a cohesive streetscape.
- **Develop a clear and consistent wayfinding system:** Well-placed signage, maps and visual cues will improve navigation, making it easier to explore local shops, amenities and public spaces along Lonsdale Avenue.
- **Enhance walkability and transit connections:** Expanding pedestrian areas, upgrading bus stops and improving crossings will make the street more transit-friendly and walkable, encouraging people to stay.
- **Preserve the intimate character of Lonsdale Avenue:** Maintain and support the street's human-scale development and diverse mix of retail to preserve its charm and identity.

Not important

Very important



Lonsdale Avenue: How can we create more accessible and user-friendly sidewalks along Lonsdale Avenue?

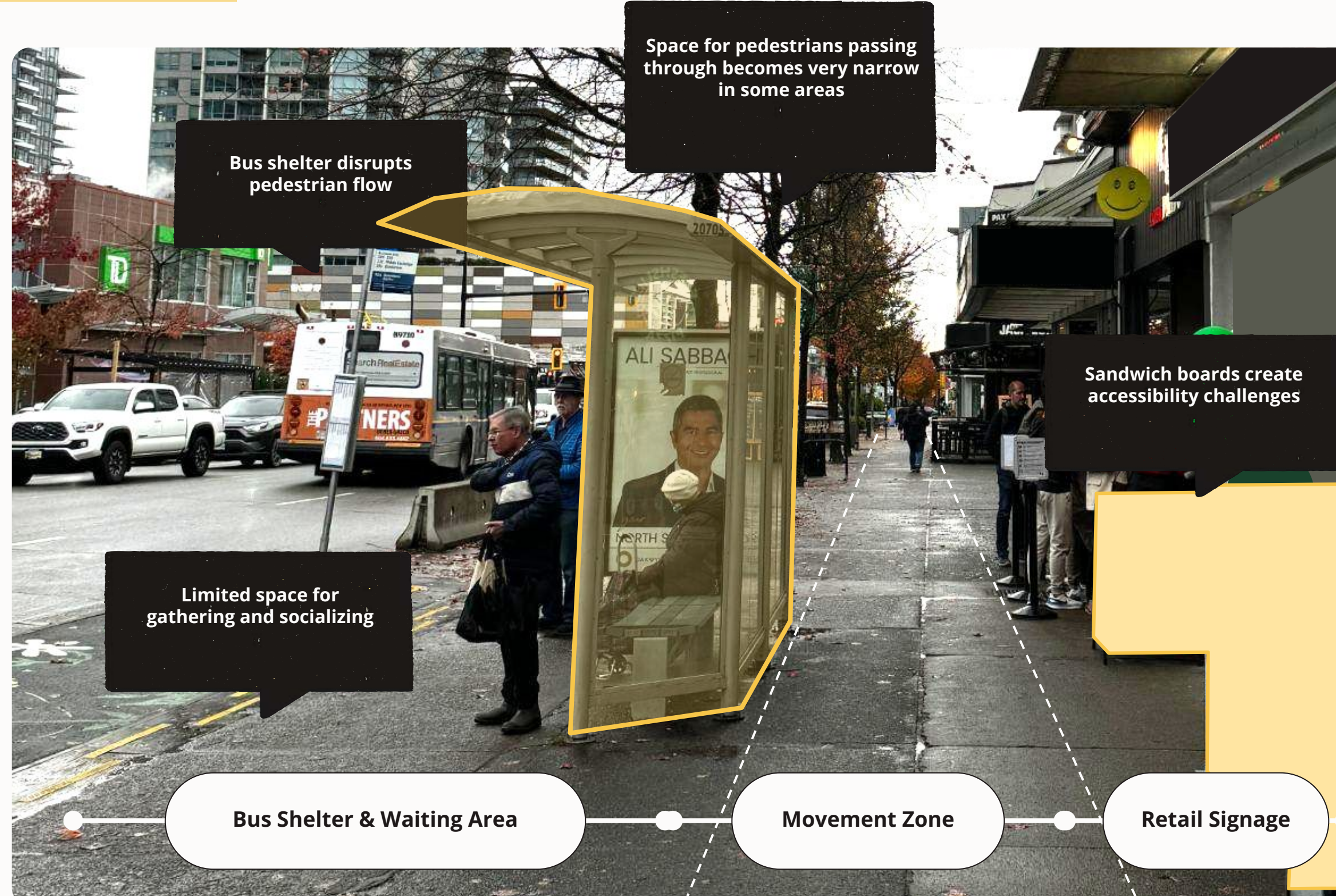
Lonsdale Avenue, like any lively main street, supports a variety of everyday activities — running errands, waiting for transit, dining on street patios, delivering goods, working in shops - and more. This mix of uses is what brings the street to life and makes it interesting. Today, however, Lonsdale Avenue struggles to accommodate all of these uses in the same space lacking clear spatial allocation. The lack of organization also limits space for socializing and greenery.

Without a defined hierarchy, different uses often clash. In some instances the placement of bus shelters crowds pedestrian flow, while retail displays create obstacles and impede accessibility. To create a more functional streetscape, a reorganization with clearly defined zones is needed, allowing these uses to coexist more harmoniously.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:



What we heard:

“I wish I could walk in a straight line without having to dodge sandwich boards or dodge unused patio fences in the winter.”

— 2025 Lonsdale Great Street Survey



41%

a TOP most desired choice

of respondents report that **more pedestrian friendly streets** would make getting to, from, and around Central Lonsdale a better experience.

In response to "Which of the following improvements would make getting to, from, and around Central Lonsdale a better experience for you?" Other top choices were convenient parking options (53%) and a more vibrant shopping and dining experience (45%).

Opportunities:

- **Define dedicated zones:** Clearly define edge zones, movement zones and furnishing zones to maximize the functionality of the street
- **Allow for flexibility:** Design adaptable areas that can accommodate different types of activities throughout the day, and across the year, such as temporary markets and events
- **Prioritize clear and accessible pathways:** Ensure sidewalks are clear of obstacles and have wide, accessible pathways for all uses, including those using assisted-mobility devices.
- **Maintain accessible paving:** Ensure smooth, even sidewalks free of obstacles and abrupt changes in grade, and incorporate tactile paving where appropriate.
- **Create space for transit waiting areas:** Consider integrating sidewalk extensions, like bulb-outs, for bus shelters to minimize congestion and improve access.

Not important

Very important



PUBLIC REALM

Running Errands & Recreating in Central Lonsdale

Lonsdale Avenue: What kind of street furniture would you like to see?

Lonsdale Avenue and adjacent open spaces have pockets of well-used seating that invite people to rest, socialize and people-watch. These are defined areas that pair seating with amenities, encourage conversation ('talkscapes') and offer interesting views. They also provide a baseline level of comfort by reducing exposure to rain, wind, glare, noise and pollution— key to making spaces inviting.

While Lonsdale Avenue has plenty of seating, much of it is poorly placed— isolated from interesting views, exposed to traffic noise and fumes, and lacking comfort features like backrests or warm, tactile materials such as wood. Overall, the Open Streets parklets are underutilized, with their success largely dependent on their proximity to amenities. Thoughtful placement and design can enhance seating areas, making them more inviting and functional, and increasing the amount of time people spend on Lonsdale Avenue.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

Different types of seating can invite diverse user groups.

Inviting people to spend time on the street means more local dollars spent along Lonsdale Avenue.

16% (17 count) Well placed
58% (69 count) Adequately placed
26% (27 count) Poorly placed

What we heard:

40% a TOP most desired choice would like to more places to sit and rest along Lonsdale Avenue.

In response to "What features would you like to see along Lonsdale Avenue?" Other top choices were street trees and plantings (63%) and public art (34%).

“ Even though there has been an attempt to add curbside seating areas, it's not a particularly inviting area to sit and hang out.”
— 2025 Lonsdale Great Street Survey

Opportunities:

- **Provide diverse and inclusive seating options:** Ensure a range of public seating types, from fixed benches with backrests to moveable chairs and seating edges.
- **Create opportunities for socializing:** Group seating in clusters to allow for both solo and group interactions.
- **Colocate seating with amenities:** Strategically locate seating near appropriate uses like cafes, cultural centres, transit areas and civic uses.
- **Address basic and evolving needs:** Offer essential amenities such as drinking water and public washrooms. Consider including amenities like WiFi and charging stations.
- **Mitigate noise and emissions:** Introduce plantings and trees as natural buffers.
- **Reimagine Open Streets parklets:** Improve their design, co-locate them with amenities.

Not important Very important

Lonsdale Avenue: How can we make evenings and all seasons more enjoyable?

With North Vancouver experiencing rain on average 150 days a year—almost half the time—Central Lonsdale must embrace all weather conditions, from wind and rain to sunshine, to truly reflect a weather-ready city.

While some buildings feature canopies or awnings, the majority of the public realm is not yet designed to protect people from the elements. Similarly, while vehicular traffic light fixtures and building lighting provide some illumination, there is a noticeable lack of pedestrian-focused, human-scaled lighting, leaving dark pockets or creating harsh contrasts. Combined with only 24% of businesses staying open past 8pm, the area's capacity to draw people to the street during evening hours is limited. The area's potential for vibrant, year-round activity, day and night, remains untapped.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

Some areas rely on light from ground-floor businesses to illuminate the public realm.

Well-lit, but only from ground-floor retail

Poorly lit, dark pockets

Some blocks are only lit by vehicular traffic lights.

What we heard:

56% a TOP most desired choice of respondents feel it is important that Central Lonsdale is vibrant and active year-round.

In response to "Which of the following statements about Central Lonsdale are important to you?" Other top choices were it is safe and welcoming for everyone (56%) and it is lively with business and commerce (40%).

20% would like to see better street lighting along Lonsdale Avenue.

In response to "What features would you like to see along Lonsdale Avenue?" Other top choices were street trees and plantings (63%) and places to sit and rest (40%).

“ It would be nice to have some nice evening spaces, maybe a space that has live music, bingo nights, trivia, movie nights, just a place where people can connect without the pressure of having to buy something.”
— 2025 Lonsdale Great Street Survey

Respondents visit Central Lonsdale across all season:
95% Summer 89% Fall
92% Spring 83% Winter

Opportunities:

- **Provide weather protection:** Install awnings, canopies, and sheltered spaces for year-round comfort.
- **Design for flexibility:** Create adaptable spaces that can cater to different needs and uses throughout the day and across seasons.
- **Enhance human-scale lighting:** Employ lighting to improve visibility, sense of safety and create a welcoming evening atmosphere.
- **Support an evening economy:** Encourage businesses to extend operating hours.
- **Promote outdoor gatherings:** Create weatherproof spaces for everyday socializing and larger events.
- **Seasonal and evening activity:** Provide space for seasonal and evening programming, like winter markets, festivals and movie nights.
- **Diversify retail offerings:** Encourage a greater mix of businesses, that extend Lonsdale Avenue's appeal beyond daylight hours.

Not important Very important



PUBLIC REALM

Running Errands & Recreating in Central Lonsdale



Lonsdale Avenue: How can we make buildings and public spaces work better together?

The relationship between building edges and the street greatly impacts the feel of Lonsdale Avenue. While small businesses contribute to vibrant, human-scale building edges, surface parking lots and blank walls leave inactive gaps that reduce pedestrian interest and engagement. Active facades, experienced at eye-level, not only feed people's need for stimuli – but they often offer invitations for people to linger.

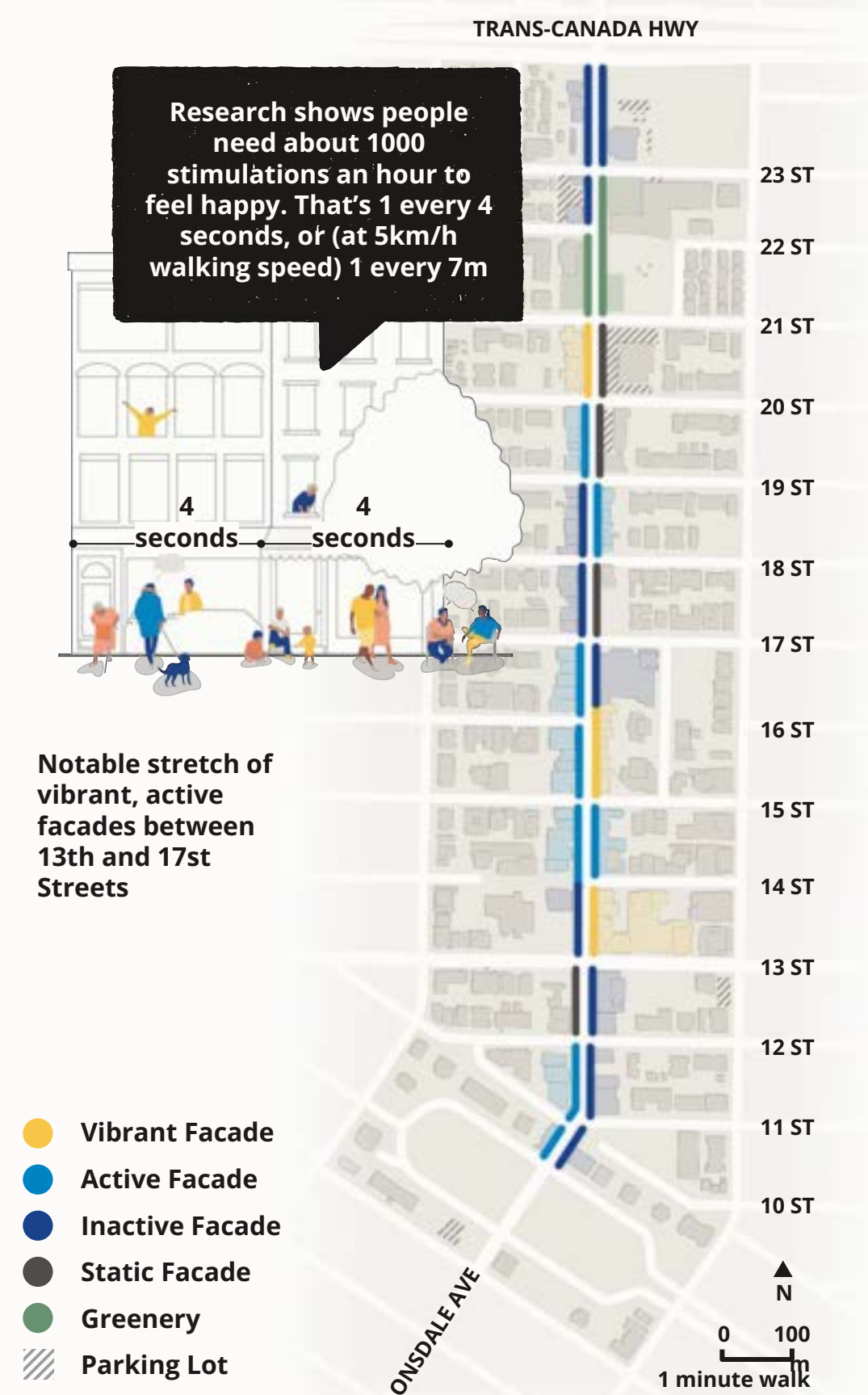
Currently, only a few blocks feature consistently active facades, most notably between 13th and 17th Streets where storefronts connect interior spaces to the street, and retail activity spills onto the sidewalk, enriching the streetscape experience. Strengthening the connection between building edges and the public realm will support a more dynamic street.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:

A rhythm of active facades, experienced at eye-level, not only satisfy people's need for stimuli — they offer invitations for people to linger and engage with local businesses on Lonsdale Avenue.



What we heard:

a TOP most important statement
48% feel it is important that Central Lonsdale is **lively with businesses and commerce.**

26% feel it is important that Central Lonsdale's **Character is preserved and enhanced.**

In response to "Which of the following statements about Central Lonsdale are important to you?" Other top choices were it is vibrant and active year round (56%) and it is safe and welcoming for everyone (56%).

Survey respondents reported their favorite things about Central Lonsdale today:

“The many small mom and pop shops and markets.”
– 2025 Lonsdale Great Street Survey

“Central Lonsdale is vibrant and bustling, with a mix of restaurants, cafes, and shops that reflect the cultural diversity of North Vancouver.”
– 2025 Lonsdale Great Street Survey

Opportunities:

- **Ground-floor transparency:** Promote facade transparency in new developments to foster a connection between indoor and outdoor spaces.
- **Operable facades:** Encourage businesses, especially within new developments, to integrate operable windows that open up to the sidewalk.
- **Incentivize facade upgrades:** Encourage businesses to enhance their facades with grants, design assistance, or other incentives.
- **Integrate public art and greenery:** Transform blank facades with murals, public art, or green walls to add visual interest.
- **Activate side streets:** Draw interest and enliven the corridor by activating side streets with retail, food & beverage offerings or interactive art.
- **Ground floor design guidelines:** Establish clear standards for ground floor development, prioritizing transparency, activity, pedestrian amenities, and street integration.

Not important

Very important



Lonsdale Avenue: What makes a great amenity?

A diverse mix of neighbourhood-serving amenities shapes public life along Lonsdale Avenue in different ways. Some ground floor uses, like restaurants, cafés, barbershops, and small-scale retail shops, actively engage the street—offering visual interest, opportunities for people-watching, and invitations to linger.

Other ground floor uses along Lonsdale Avenue, including banks, pharmacies, and health clinics, are contributing less to street vitality today. Setback entrances and opaque facades create inward-focused spaces, missing opportunities to connect with passersby.

Better regulation of ground-floor uses can encourage more street-facing functions that foster an engaging and dynamic streetscape.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

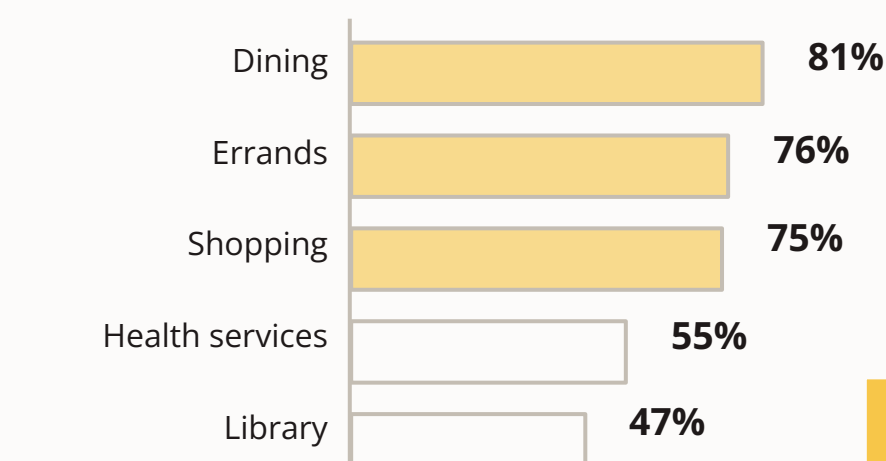


What we saw:



What we heard:

Why do you typically visit:



“Should restrict banks, money exchanges, pharmacies, dentists and doctors from ground floor space. This space should be for local shops.”
– 2025 Lonsdale Great Street Survey

“The library and civic plaza is the best public space in Central Lonsdale. Community activations there are great”
– 2025 Lonsdale Great Street Survey

Opportunities:

- **Develop a set of ground floor guidelines:** Prioritize ground-floor functions along Lonsdale Avenue that actively engage with the streetscape, and limit those that do not.
- **Open up arts & cultural institutions:** Encourage galleries and theatres at street level to turn their buildings “inside out” to express their values and program on the street.
- **Expand outdoor programming:** Support temporary or permanent sidewalk activations, like café patios, pop-up markets, and cultural events that spill into the public realm.
- **Support outdoor dining:** Allow more space for sidewalk cafés and curbside patios, encourage heated patios, wind barriers and covered seating to extend it year-round.

Not important

Very important



PUBLIC REALM

Running Errands & Recreating in Central Lonsdale

Central Lonsdale: How can we create public spaces for all ages and interests?

Central Lonsdale presents a unique demographic composition. While the average age is relatively young at 46, the area houses a higher proportion of seniors aged 85+ compared to neighbouring areas. Furthermore, 36% of families have children, contrasting with the 48% provincial average. This distinct makeup calls for a network of inclusive spaces that offer opportunities for a diverse range of ages and interests.

While Lonsdale Avenue acts as a vital spine, it cannot serve all needs on its own. Central Lonsdale lacks a cohesive network of public spaces that complement the streetscape and serve different users—spaces where children can play freely, teenagers can socialize, and seniors can relax and connect with their community. Activating underutilized laneways presents an opportunity to provide additional public spaces that help fill this gap, working to support an "age-friendly" city where everyone feels a sense of belonging.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we heard:

33% a TOP most desired choice would like to see more park / play areas in Central Lonsdale.

In response to "What kind of amenities would you like to see more of along Central Lonsdale?" Other top choices were local businesses and shops (65%) and food and beverage offerings (52%).

25% a TOP most desired choice respondents stated their experience in Central Lonsdale would be improved by more public spaces and plazas.

In response to "Which of the following improvements would make getting to, from, and around Central Lonsdale a better experience for you?" Other top choices were convenient parking options (53%) and a more vibrant shopping and dining experience (45%).

“There is lots of hiking, walking and sport fields but not a lot of spaces where families and friends can just hang out outdoors. I wish there were some nice parks for sitting, reading and enjoying a nice day.”
- 2025 Lonsdale Great Street Survey

“The library and civic plaza is the best public space in Central Lonsdale”
- 2025 Lonsdale Great Street Survey

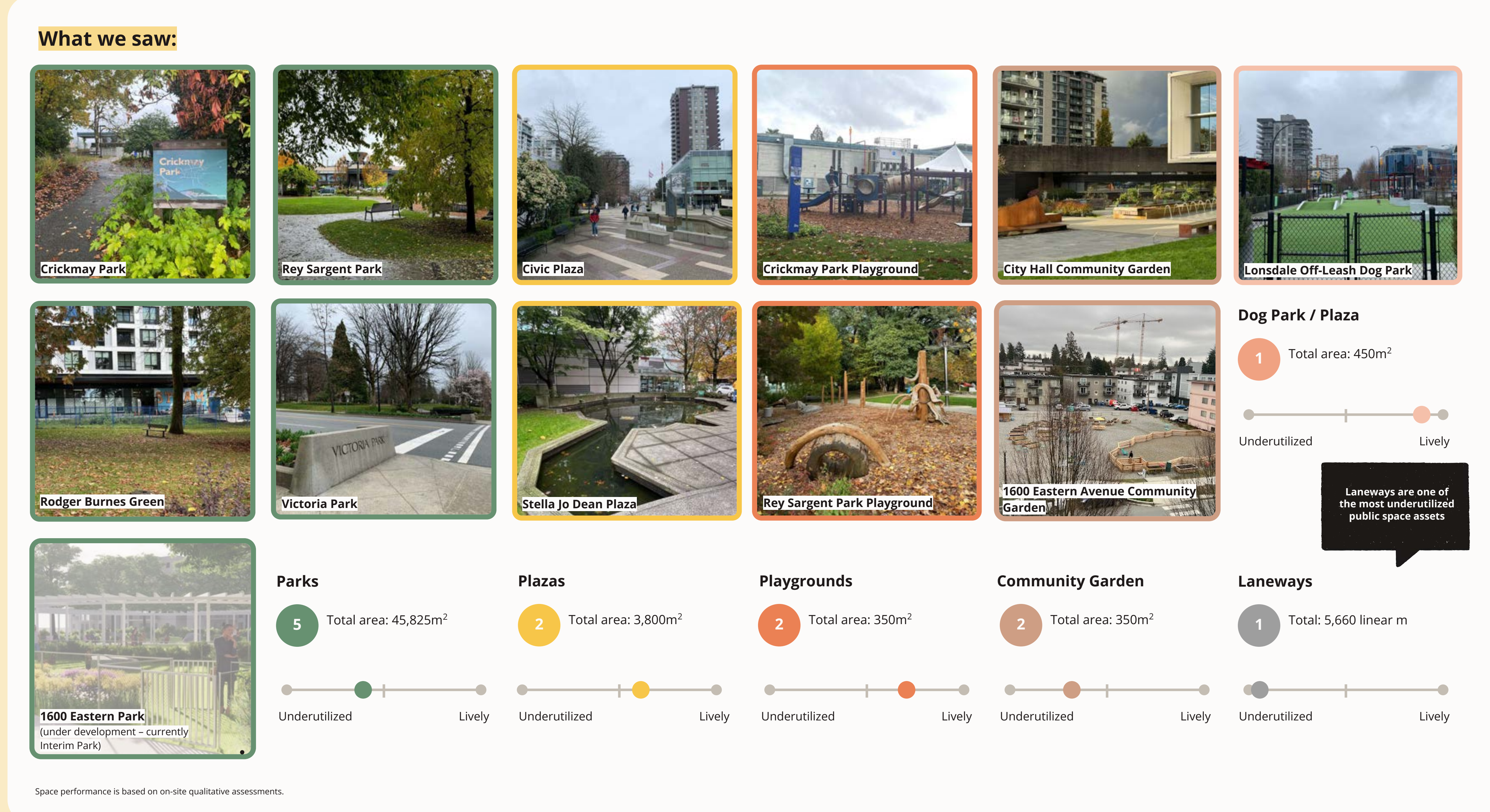
“Make better and more frequent use of Civic Plaza for artistic and cultural events”
- 2025 Lonsdale Great Street Survey

- Opportunities:**
- **Create a robust network of public spaces:** Establish a well connected and diverse network of public spaces by leveraging underutilized spaces, like laneways, or setbacks.
 - **Design for all ages:** Create spaces with amenities and features that appeal to various age groups, from children and teens to seniors and those with mobility challenges.
 - **Integrate play elements:** Incorporate interactive features, such as musical installations or climbable seating, that encourage exploration.
 - **Provide youth-oriented spaces:** Offer dedicated social gathering areas and programming tailored to the needs and interests of teenagers.
 - **Prioritize accessibility:** Ensure comfortable and accessible seating options for seniors and individuals with mobility-assisted devices.
 - **Provide diverse programming:** Host events, such as outdoor movie nights, live music, or fitness classes, that appeal to a wide range of age groups.



Central Lonsdale: Types of public spaces in the neighbourhood

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.





LANDSCAPE

Connecting to Nature & Cultural Heritage

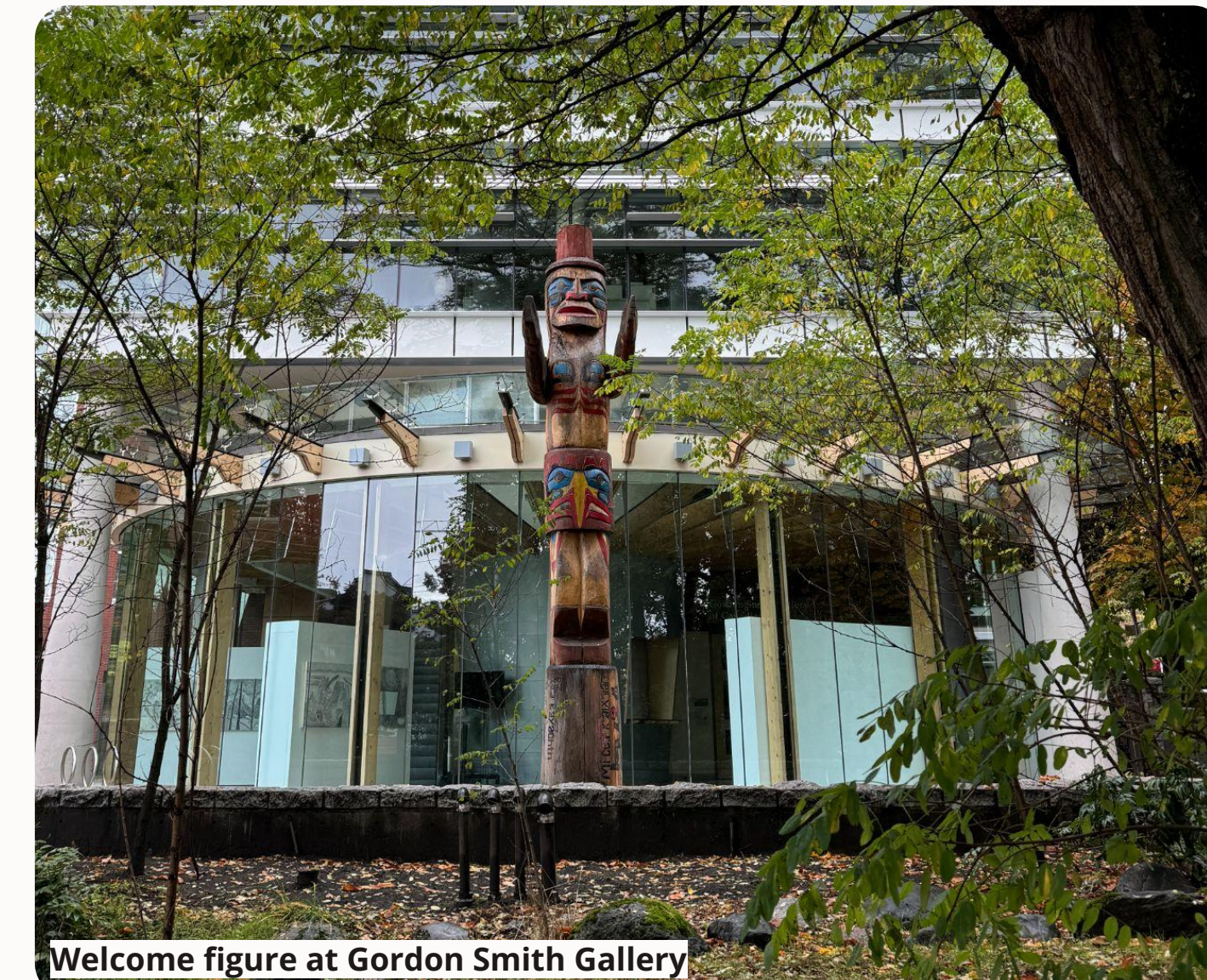
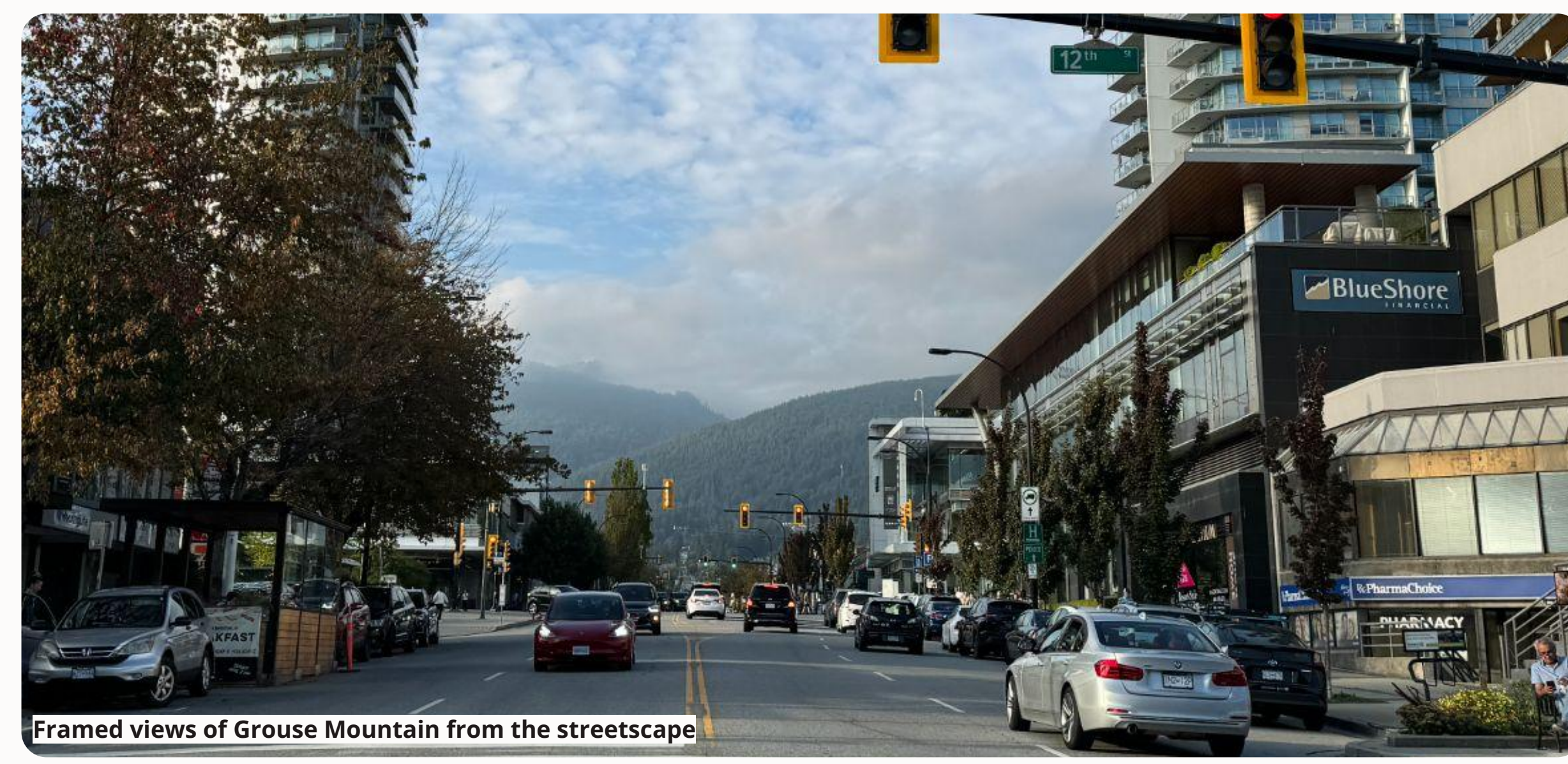


Central Lonsdale: How can we celebrate our natural and cultural heritage?

Nestled amidst a stunning natural landscape with dynamic topography that affords views of the North Shore mountains and Burrard Inlet, Central Lonsdale is a unique place. Though, present views to the surrounding landscapes are seldom along Lonsdale Avenue, and often obstructed by buildings and tree canopies.

The region is layered with a rich cultural heritage of the Skwxwú7mesh (Squamish) and Səlilwataʔ/Selilwitulh (Tsilil-Waututh) Nations, still active in the community today, and also home to a significant Iranian population (17.3%). Despite this cultural diversity, indigenous and cultural expression in the public realm is limited, leaving room for improved connections to the both environmental and cultural contexts.

What we saw:



What we heard:



21%

a TOP most desired statement
feel it is important that Central Lonsdale reflects and celebrates local communities and cultures.
In response to "Which of the following statements about Central Lonsdale are important to you?" Other top statements were it is vibrant and active year round (56%) and is it safe and welcoming for everyone (56%).

“ Let’s make the most of our amazing location. That’s what differentiates us from other communities. I don’t think we want to be a stylish but generic shopping street.”
— 2025 Lonsdale Great Street Survey

Opportunities:

- **Celebrate ecological context:** Provide opportunities through art, sensory installations and view corridors to connect with local ecology.
- **Incorporate native plantings:** Integrate native plants into streetscapes and green spaces to restore the native biome and create a stronger sense of place.
- **Incorporate cultural elements:** Partner with Skwxwú7mesh (Squamish) and Səlilwataʔ/Selilwitulh (Tsilil-Waututh) Nations to incorporate cultural elements, such as native gardens, artwork and indigenous knowledge into public spaces.
- **Cultural visibility:** Highlight cultural diversity through public art and relevant events.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important

Very important

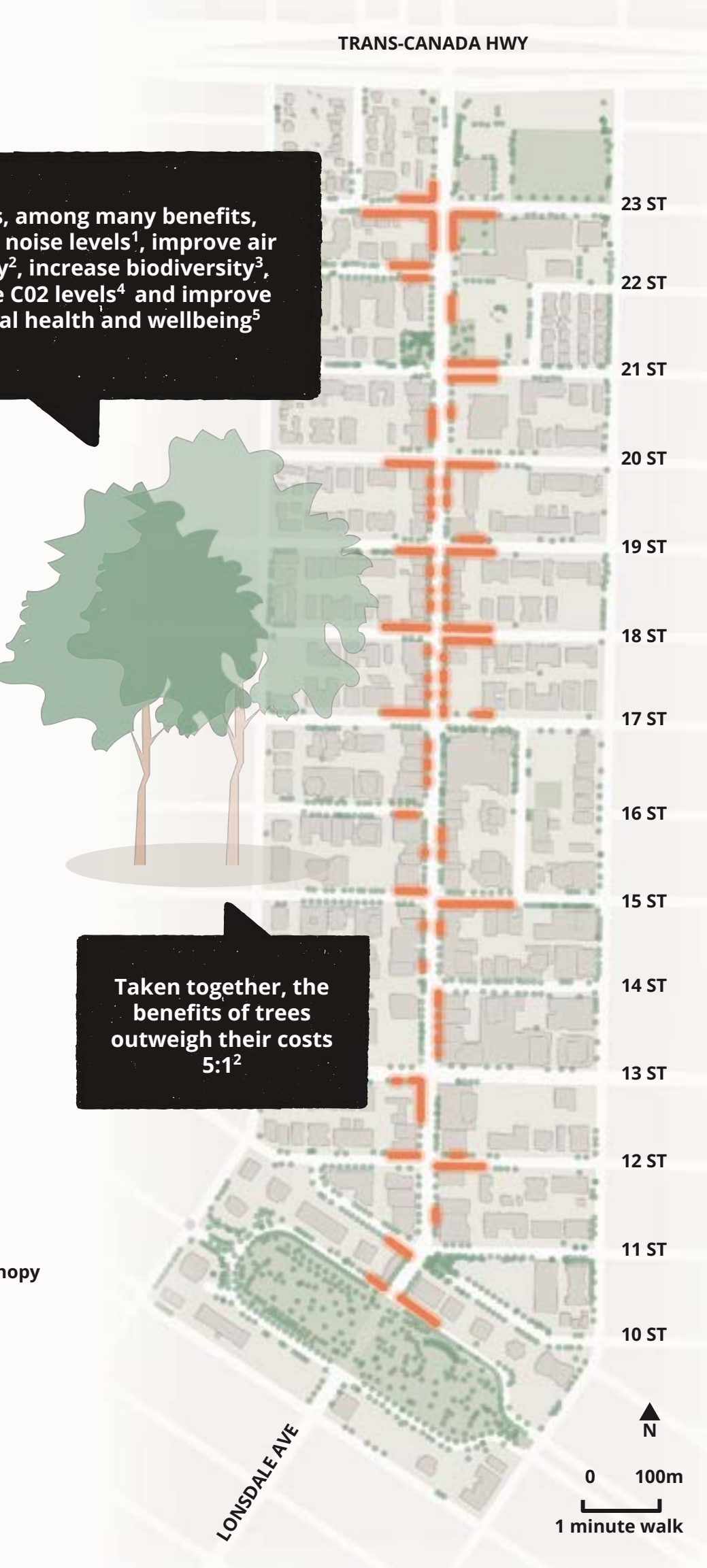
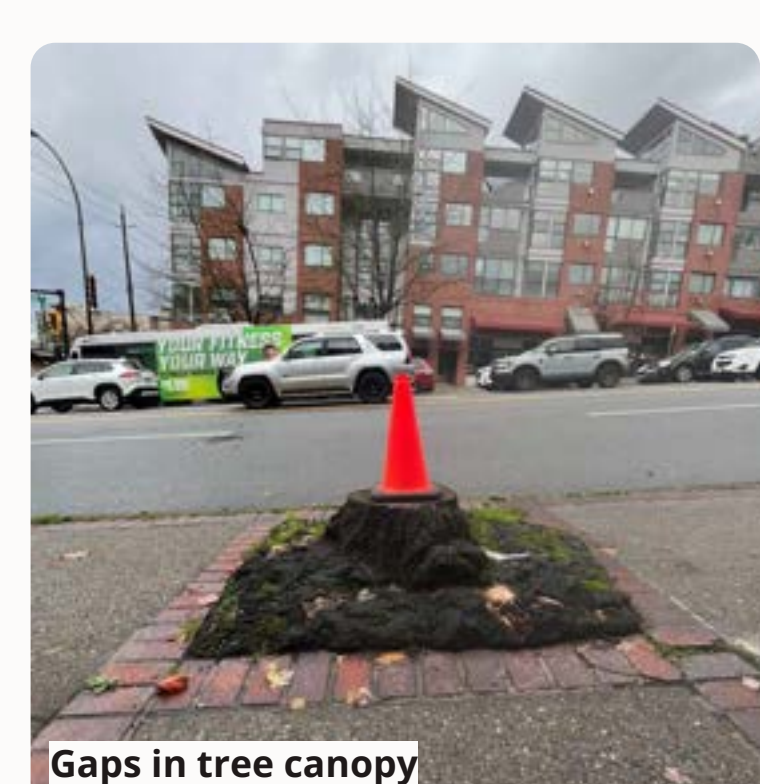


Lonsdale Avenue: Planting, care, and growth – How can we support a healthy tree canopy?

Urban forests are essential for climate resilience – they maintain habits, preserve biodiversity and reduce urban heat island effects. Street trees in Central Lonsdale are also an essential component of the public realm, enhancing the pedestrian experience by providing shelter from harsh sunlight and rain, improving air quality, and absorbing noise pollution.

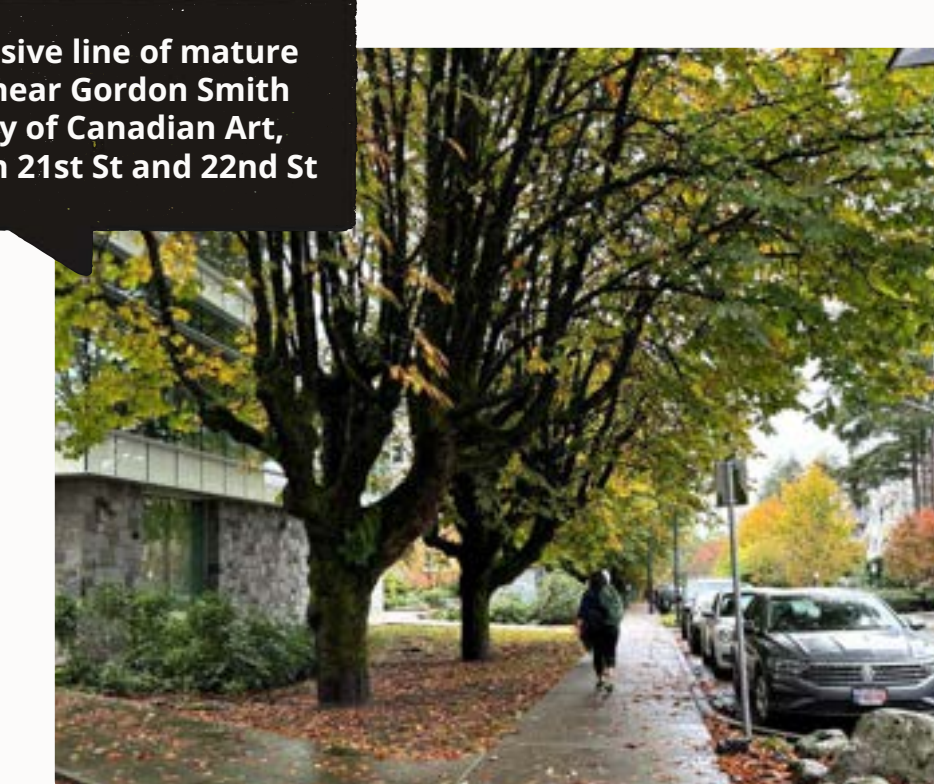
However, with 58% of trees in poor or declining condition, the current canopy cover of 16% falls short of the City’s goal of 24% by 2035. Additionally, columnar tree species and restrictions imposed by underground utilities contributes to significant gaps in canopy cover along the street leaving pedestrians exposed to weather, fumes and noise.

What we saw:



But!

There are some incredible mature trees already along, and around, Lonsdale Avenue.



Trees, among many benefits, reduce noise levels¹, improve air quality², increase biodiversity³, reduce CO2 levels⁴ and improve mental health and wellbeing⁵.

Taken together, the benefits of trees outweigh their costs 5:1⁵.

What we heard:



21%

a TOP most desired choice
feel it is important that Central Lonsdale reflects and celebrates local communities and cultures.
In response to "What features would you like to see along Central Lonsdale?" Other top choices were places to sit and rest (40%) and more public art (34%).

41%

a TOP most desired statement
feel it is important that Central Lonsdale is lush with trees and greenery
In response to "Which of the following statements about Central Lonsdale are important to you?" Other top statements were it is vibrant and active year round (56%) and is it safe and welcoming for everyone (56%).

Opportunities:

- **Prioritize tree care:** Implement a comprehensive tree care program that includes regular inspections, pruning, and disease management to address street trees in poor or declining health.
- **Expand planting opportunities:** Utilize the generous laneways and potential new pocket parks to increase canopy cover and create a more connected urban forest.
- **Select appropriate species:** Prioritize trees with larger, rounded canopies to provide more shade and weather protection, while favoring species native to the region to restore habitats and biodiversity.
- **Foster community stewardship:** Partner with local organizations, First Nations, and residents to establish volunteer tree care programs, community gardening initiatives, and educational workshops that promote environmental awareness and responsibility.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important

Very important



LANDSCAPE

Connecting to Nature & Cultural Heritage

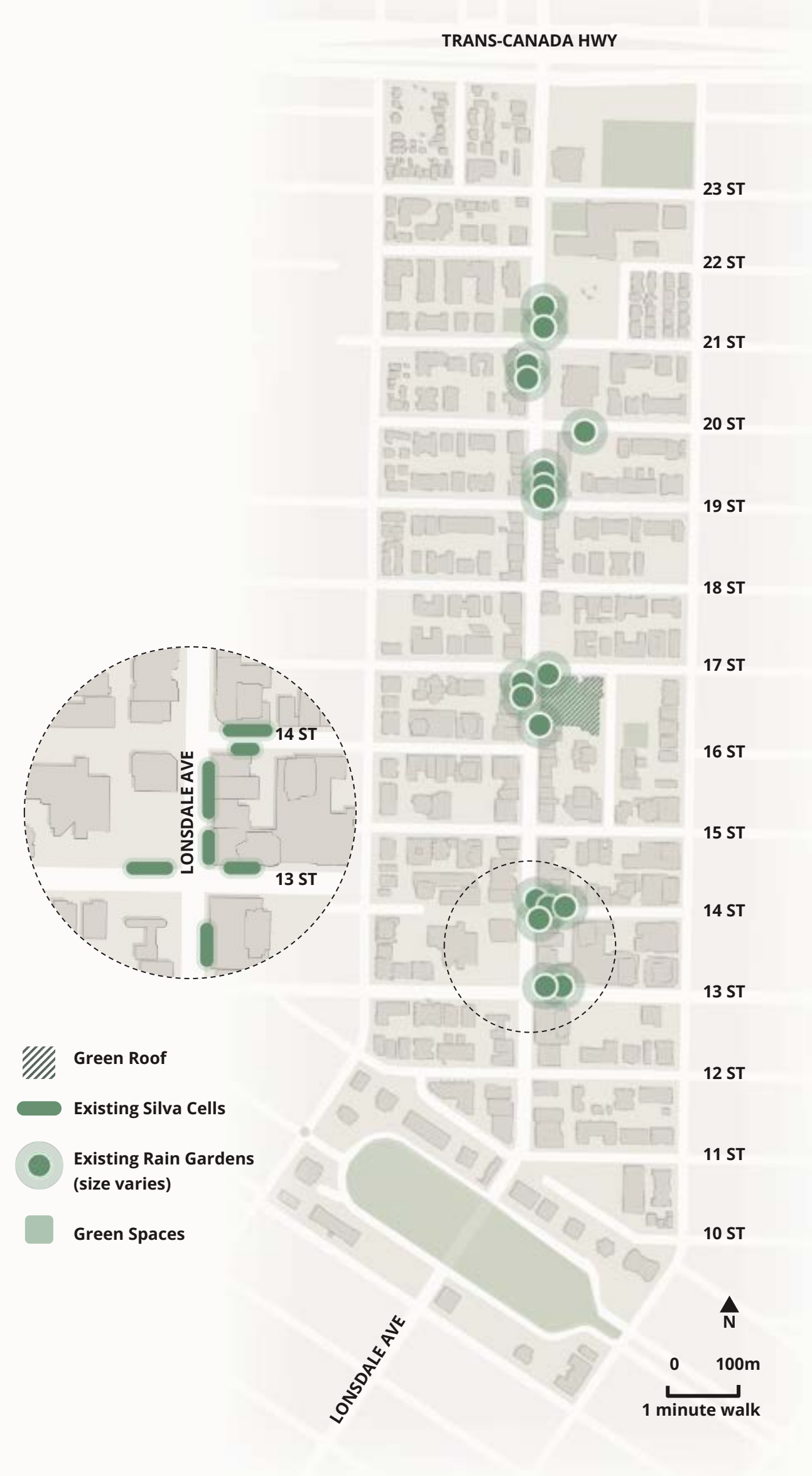


Lonsdale Avenue: How can we protect the Wagg Creek Watershed?

Central Lonsdale is situated within the Wagg Creek watershed – the most dominant drainage area in the City of North Vancouver and one susceptible to the region's recent increase in flooding.

17 rain gardens and one green roof on Lonsdale Avenue provide 3,630m² of permeable surface and filter rainwater onsite, contributing to the City's goal to improve stream and riparian health and water quality. However, the majority of Lonsdale Avenue remains impervious, as a result sending untreated stormwater directly into the watershed.

What we saw:



What we heard:

“I love the rain gardens that go up Lonsdale Avenue.”
– 2025 Lonsdale Great Street Survey

“I would like improved drainage. When it rains, the streets become flooded and very difficult to cross.”
– 2025 Lonsdale Great Street Survey

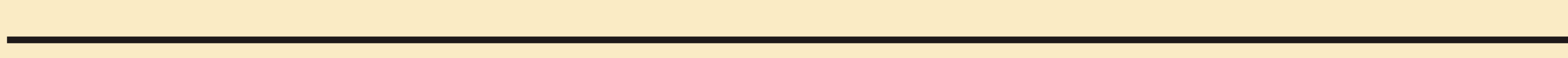
Opportunities:

- **Maximize green infrastructure:** Incorporate rain gardens, bioswales, and green roofs to capture and treat rainwater on site, working to integrate these features with existing underground utilities.
- **Optimize existing rain gardens:** Enhance the performance and aesthetic appeal of existing rain gardens through regular maintenance, targeted plant selection, and design improvements.
- **Explore pervious surfaces:** Investigate the feasibility of using permeable paving materials in sidewalks and plazas to reduce untreated runoff and enhance groundwater recharge.
- **Optimize the right-of-way:** Explore opportunities to reduce on-street parking in strategic locations to create space for rain gardens, bioswales, and other green infrastructure elements that enhance ecological function and improve the pedestrian experience.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important



Very important



Central Lonsdale Laneways: What's the potential for our laneways?

Lonsdale Avenue is surrounded by a network of laneways that support the essential services that make day-to-day life in the neighbourhood tick – garbage pick up, delivery, drop-off.

Currently, these laneways are primarily service corridors and occasionally serve as pedestrian short-cuts. The ubiquity of these laneways provides an untapped opportunity to expand both green infrastructure and public space in the neighbourhood through the integration of pocket parks, rain gardens, safe walking routes, restaurant patios and gathering spaces.

What we saw:



Laneways are the narrow, often overlooked, streets that run behind buildings. They have the potential to become valuable community assets!



Opportunities:

- **Determine potential laneways for pedestrian-focused activation:** Create destinations along strategically chosen laneways that allow for safe pedestrian passage and an extension of programming and activity, while ensuring space for essential services to run smoothly.
- **Further support service-focused laneways:** work with institutions, business owners and residents to optimize trash pickup, delivery, drop off etc.
- **Integrate wayfinding:** Provide clear wayfinding that invites pedestrian activity into laneway walking routes and destinations.
- **Improve lighting:** to increase safety, wayfinding and inviting atmosphere in the laneways.
- **Prioritize accessibility:** keep pedestrian-focused laneways free of obstacles and uneven terrain.
- **Integrate green elements into laneways:** Introduce landscaping, green walls, and stormwater management features to enhance the ecological function and aesthetic appeal of laneways.
- **Pocket parks:** With the current traffic flows, there may be opportunities to dead-end some quieter streets to accommodate new pocket parks, leveraging laneway space.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important



Very important



PUBLIC REALM & LANDSCAPE

Tell us your thoughts & ideas!

Q

What would make Lonsdale Avenue feel more comfortable for pedestrians? What changes would make you want to spend more time there?

Q

What's missing in Central Lonsdale? What kinds of open space activities would make it a better place - in all seasons and into the evening?

Q

Is there anything else you'd like to share with us?

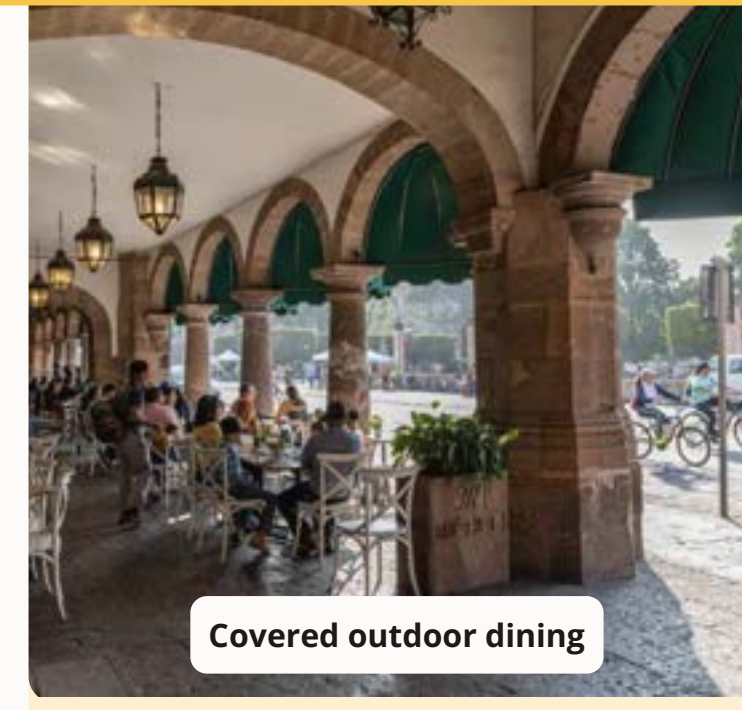
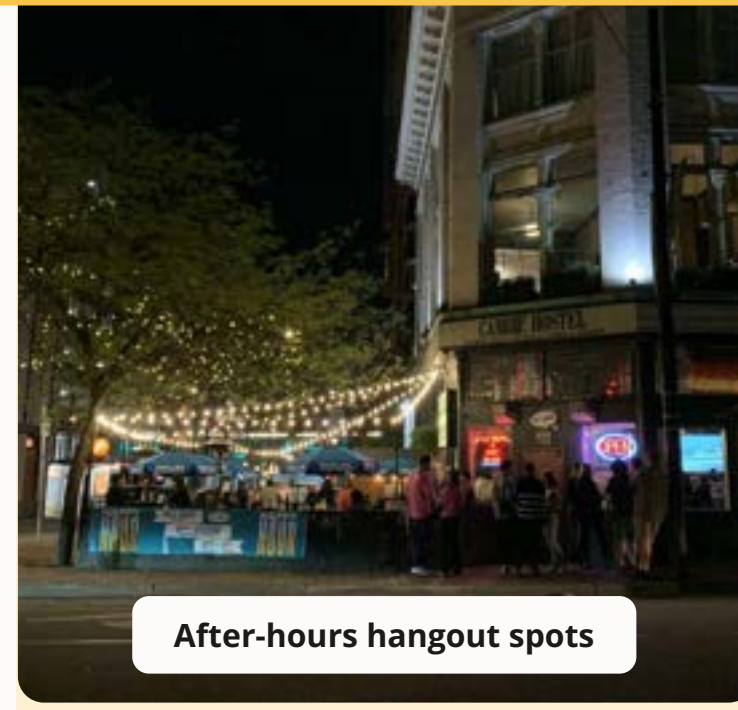
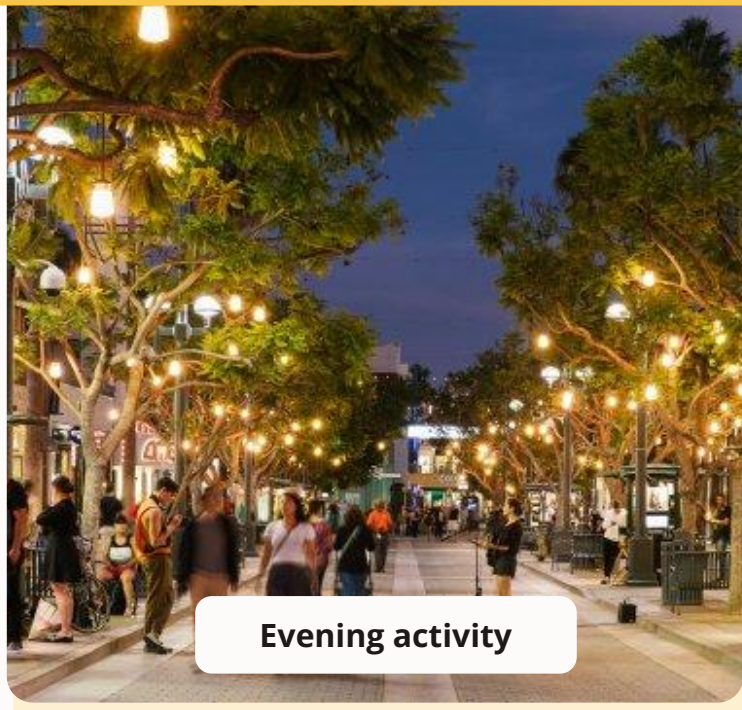




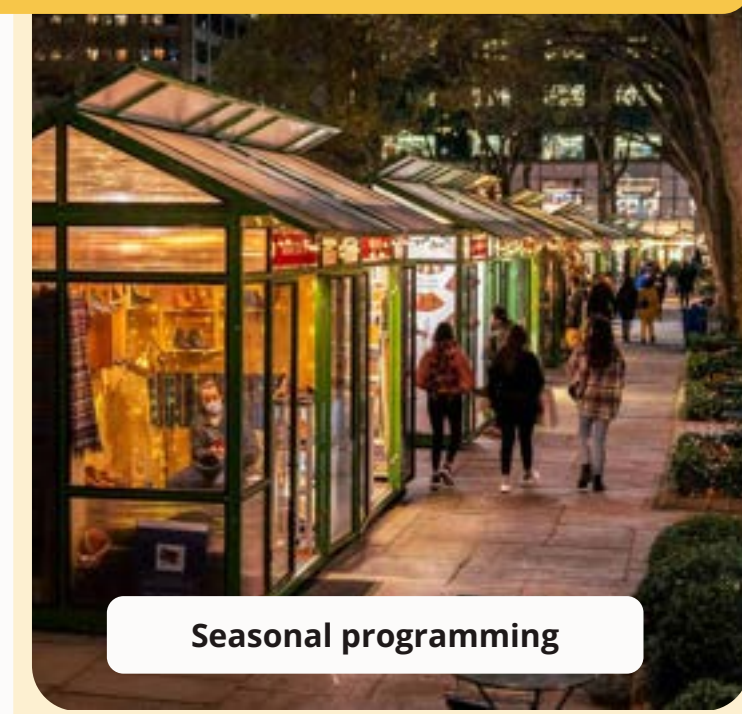
PUBLIC REALM AND LANDSCAPE

Inspiration Images

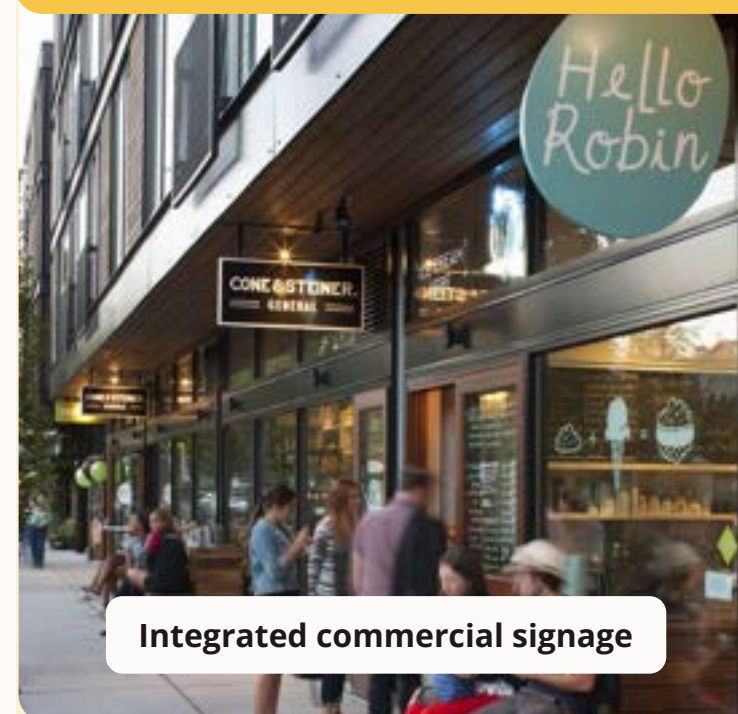
All day, all year street



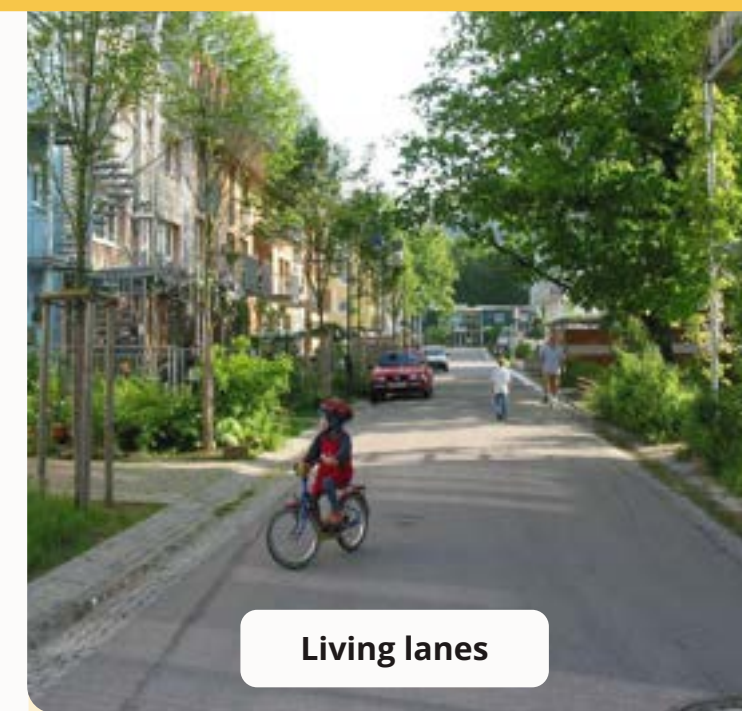
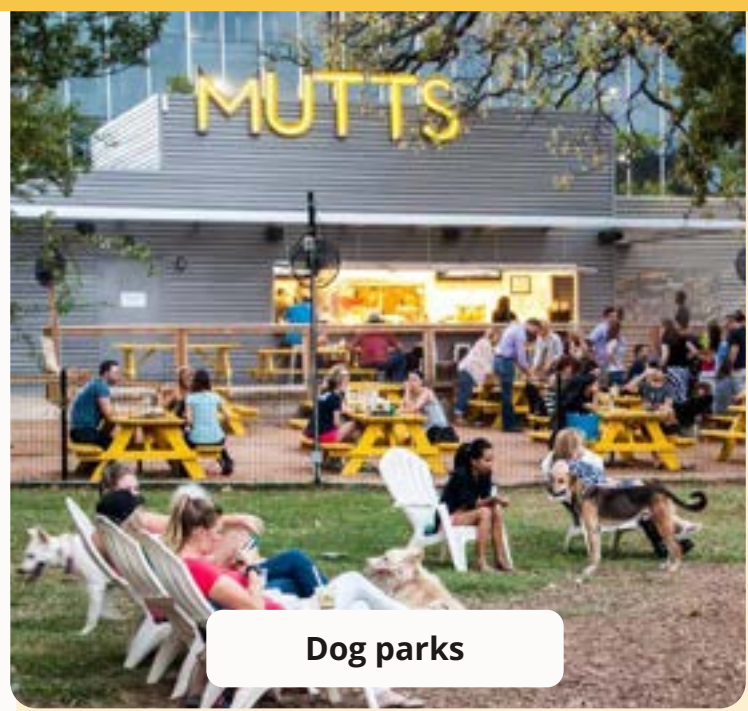
Events and programming



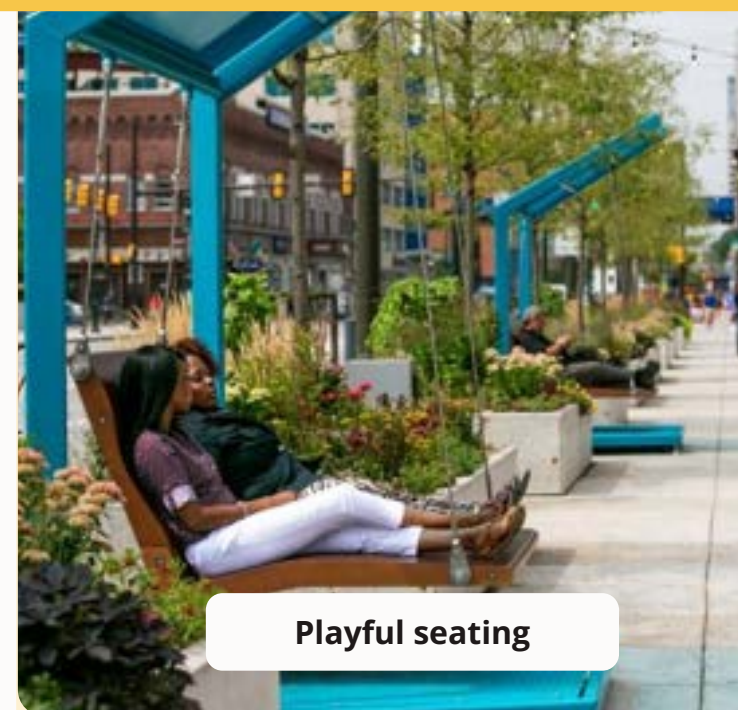
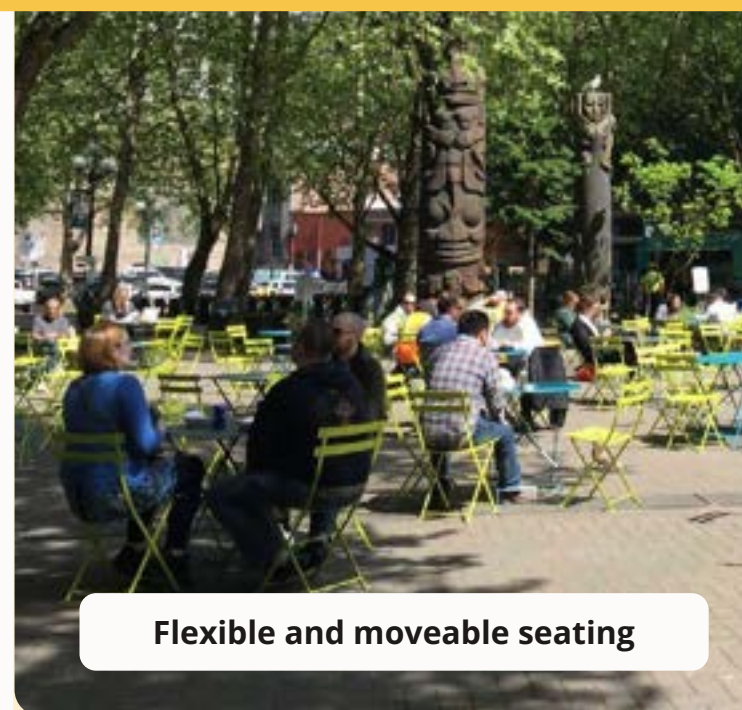
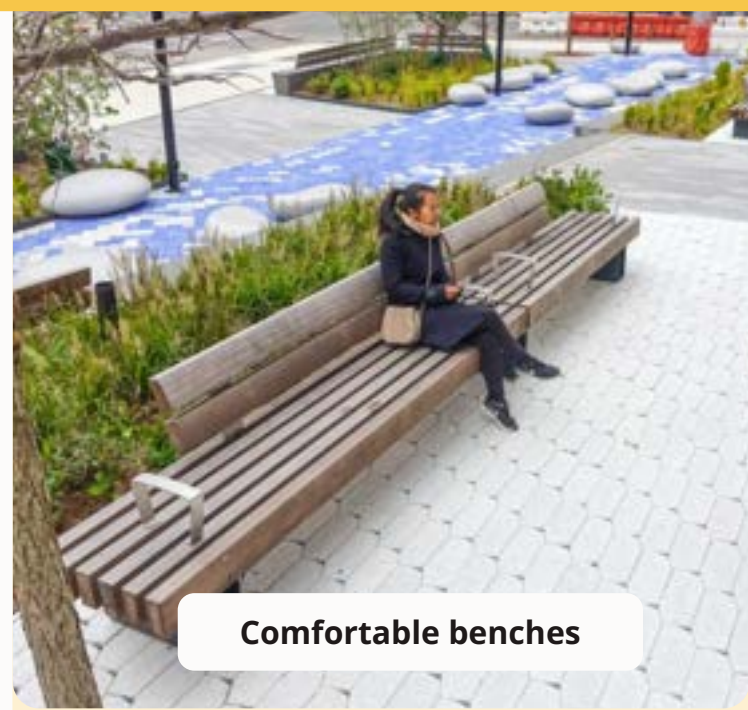
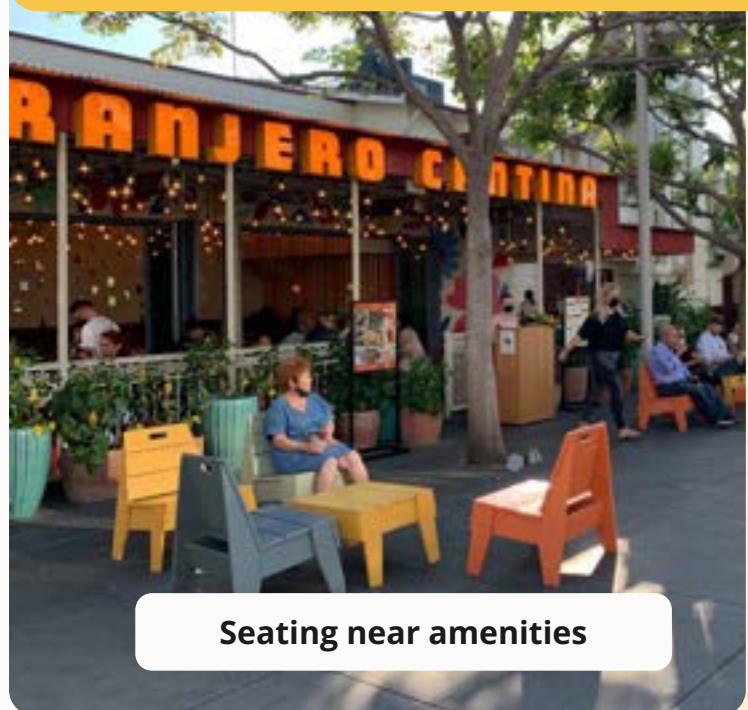
Commercial edges



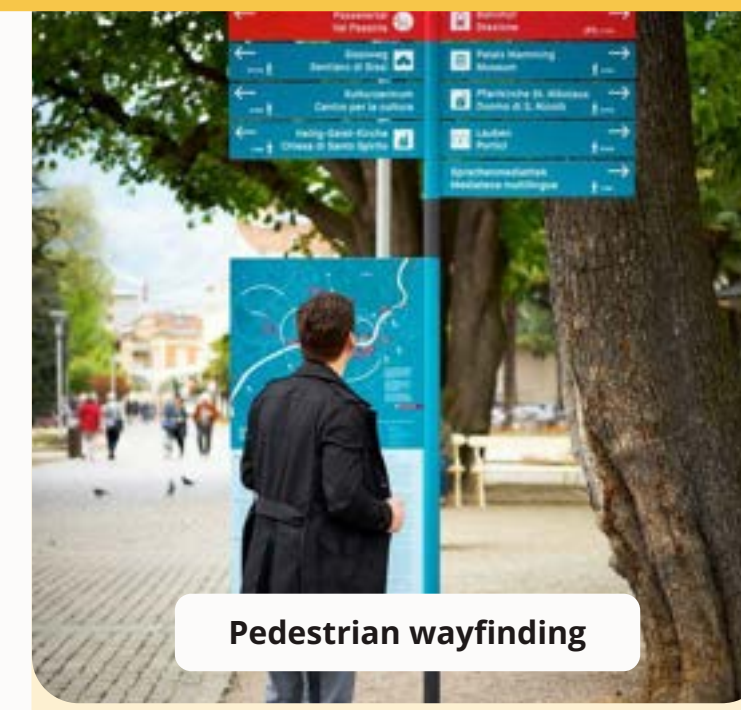
Types of public spaces



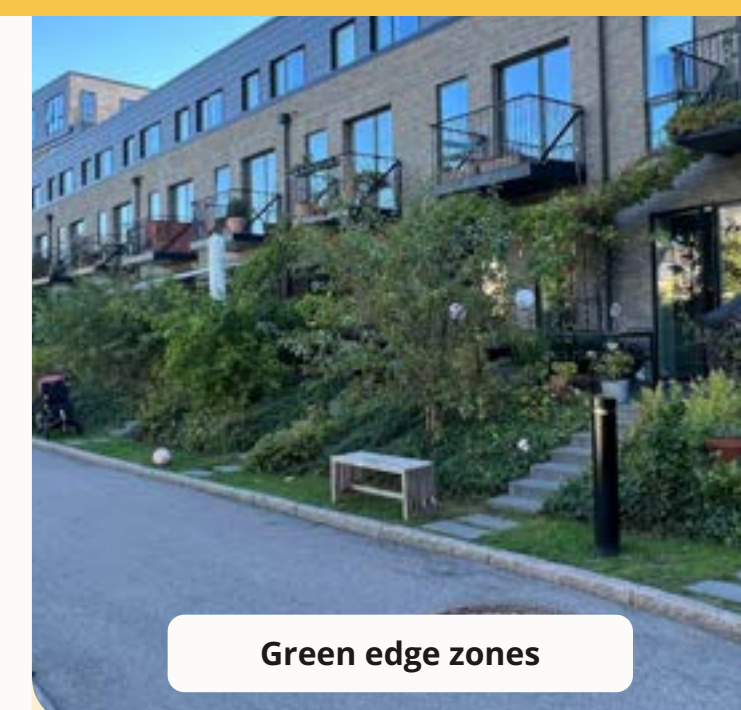
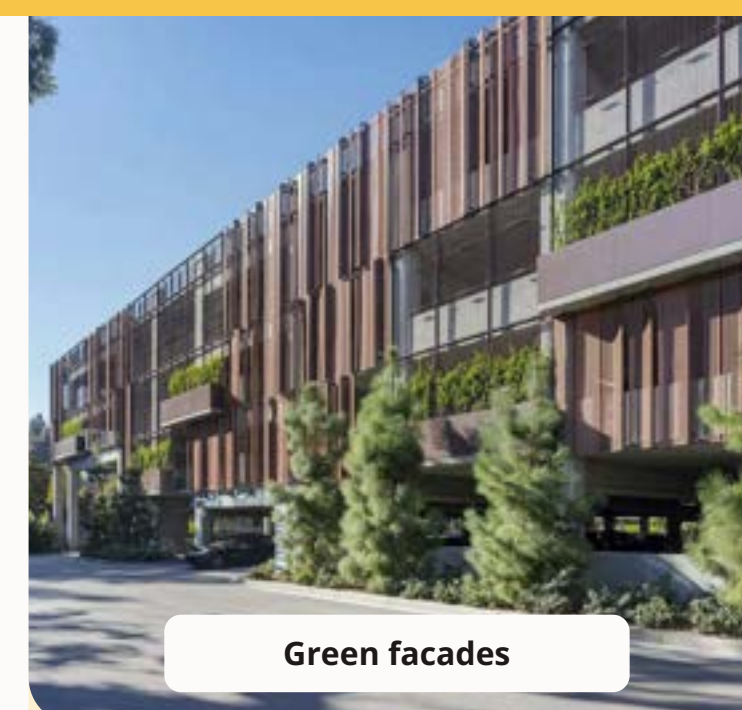
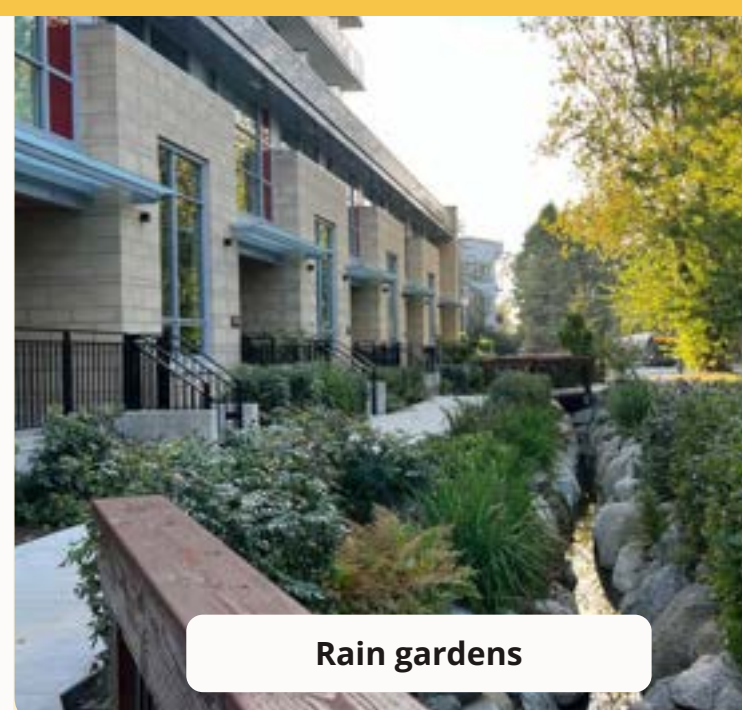
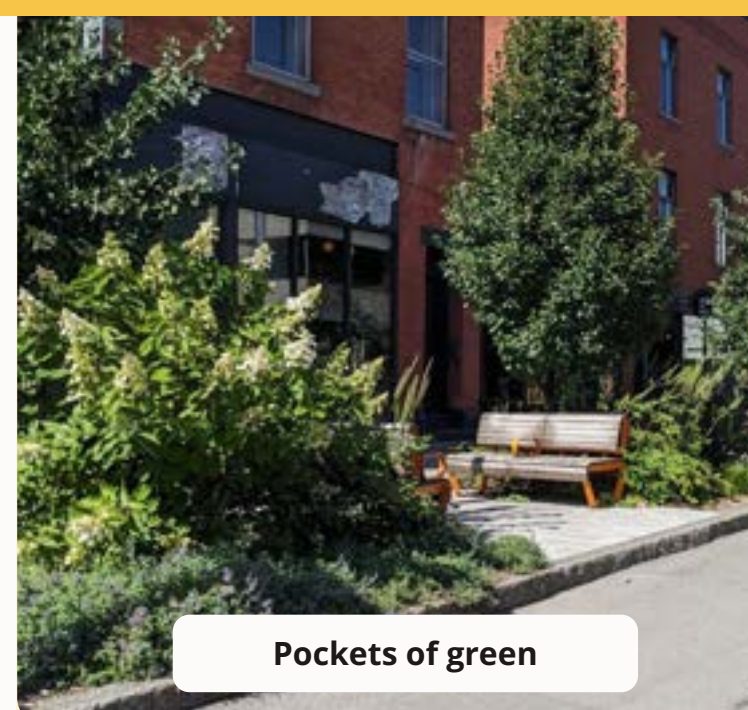
Seating



Amenities & wayfinding



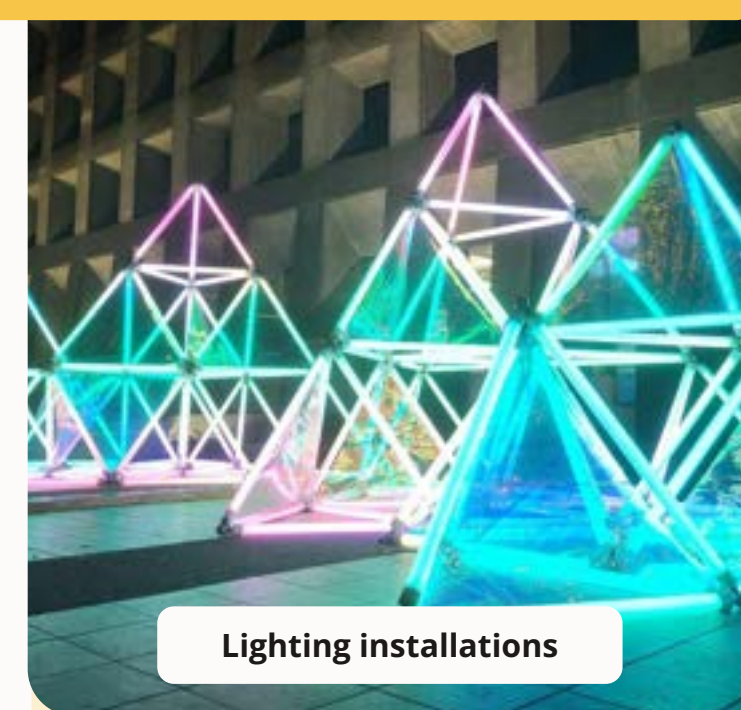
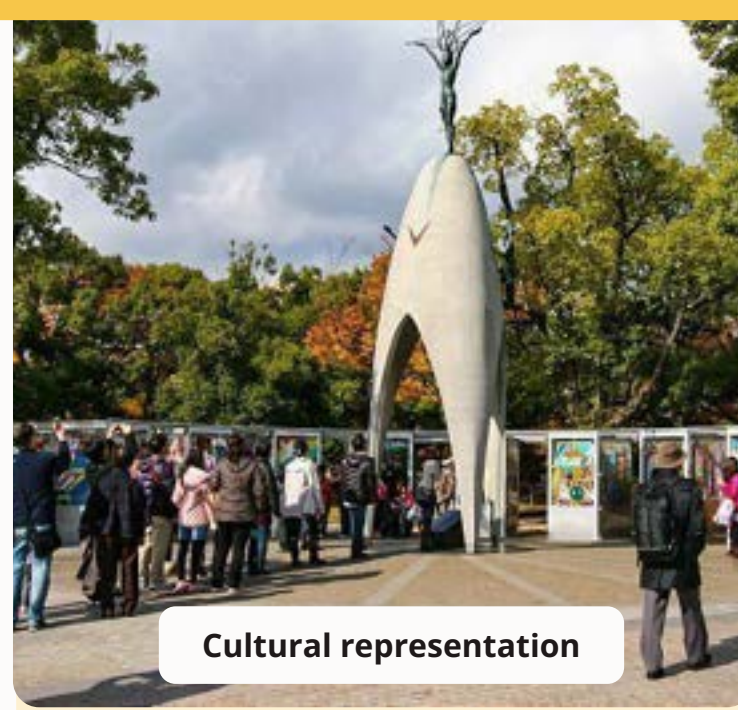
Trees, plantings and green infrastructure



Place stewardship



Public art and installations





LAND USE, PLANNING & ECONOMIC ASSESSMENT

Living & Working in Central Lonsdale

Shaping Central Lonsdale: What role will Lonsdale Avenue play?

As Central Lonsdale continues to grow, future development could reinforce this established pattern of clustered uses, strengthening the existing nodes while expanding their capacity and concentrating complementary functions. Alternatively, a networked approach would distribute amenities and commercial activity more evenly across the site, improving convenience for residents by reducing distances to everyday services, fostering mixed-use development and enhancing connectivity. Strategic planning is essential for Central Lonsdale's future growth and will shape its long-term livability and economic vitality, whether through reinforcing its existing clusters or embracing a more dispersed, networked framework.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

Cultural amenities at the northern end (Gordon Smith Gallery)

Health and civic functions at the southern end of Lonsdale Avenue

- Major anchor
- Park
- Arts, cultural institution
- Health centre
- Residential
- Commercial
- Mixed Use
- Institutional
- Industrial
- Recreational

Map labels: cultural & recreational cluster, health & civic cluster, commercial spine, Harry Jerome Community Rec Centre, City Hall, City Library and Civic Plaza, Lonsdale Ave, TRANS-CANADA HWY.

What we heard:

a TOP most important statement

48% feel it is important that Central Lonsdale is **lively with businesses and commerce.**

26% feel it is important that Central Lonsdale's **Character is preserved and enhanced.**

Q: What is your favourite thing about Central Lonsdale today?

A: "The density of small, local businesses. The fact that there are so many unique shops and it's not full of cookie-cutter brands we see everywhere - it is not a "high street" with high-end shopping, but a reflection of the local community."

- 2025 Lonsdale Great Street Survey

Opportunities:

- **Support a vibrant mixed-use spine:** Establish a lively and continuous mixed-use core by prioritizing the ground floor experience along Lonsdale Avenue and filling gaps in where facades are less active.
- **A networked experience:** Seize the opportunity to extend public life and activations beyond the linear core and develop a networked neighbourhood.
- **Harmonious transitions:** Use building height and zoning to create a seamless transition between Lonsdale and surrounding neighbourhoods.
- **Strategic site activation:** Focus redevelopment efforts on key sites along Lonsdale Avenue and within the adjacent zones to create exciting mixed-use hubs that support both the commercial spine and the surrounding neighbourhoods.
- **Enhance connectivity between activity clusters:** Reinforce Lonsdale Avenue as a central, unifying element of the community by improving pedestrian and multi-modal connections between the health and civic areas to the south, cultural and recreational areas to the north, and the residential areas in between.

Not important Very important

Can simpler planning policies create better communities?

The prevalence of Comprehensive Development (CD) Zones reinforces a site-by-site approach to planning, potentially hindering a unified vision for the corridor. Updating the Zoning Bylaw to reflect a more integrated and strategic approach for Central Lonsdale could support many goals for this area, such as better public space, livability, and more housing choices. At the same time care needs to be taken to make sure that small businesses can continue to thrive on Lonsdale even as development takes place.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

New mixed-use development with articulated facade details and ground floor retail enhances the street-level experience

Ground level setbacks at times do little to activate the street

Mid-rise and fine-grained commercial buildings vary in character

What we heard: size

a TOP most desired choice

41% feel it is important that Central Lonsdale is **more pedestrian friendly with more landscaping and amenities.**

Q: Which of the following improvements would make getting to, from, and around Central Lonsdale a better experience for you? Other top choices were convenient parking options (53%) and a more vibrant shopping and dining experience (45%).

“ More density! Central Lonsdale can be planned to allow for vibrant, ground floor retail and commercial space with residential and office spaces above while retaining view shafts and natural light.

- 2025 Lonsdale Great Street Survey

Opportunities:

- **Form-based zoning:** Form-based zoning overlays guide development and reinforce area character. A building height framework can offer greater control over urban form than FSR (Floor Space Ratio).
- **OCP density bonuses:** Density bonuses tied to community benefits (affordable housing, art, sustainability) incentivize desired development aligned with OCP goals.
- **Street activation metrics:** Linking mixed-use incentives to performance metrics (active frontage, business types, plaza design) optimizes street activation.
- **Measurable pedestrian standards:** Design standards with measurable outcomes (sidewalks, trees, facades) enhance the pedestrian realm.
- **Integrated public realm:** A city-led public realm strategy, potentially a Comprehensive Plan, creates a cohesive network of public spaces.

Not important Very important



LAND USE & PLANNING

Living & Working in Central Lonsdale



Central Lonsdale's Future Skyline: How should it look?

Building heights play a defining role in Lonsdale Avenue's character, shaping views, and influencing the pedestrian experience. While a range of building heights can support greater density and population growth, careful management is essential to preserve neighbourhood character, protect mountain and ocean views, and enhance the experience at eye-level.

As the City of North Vancouver updates its Official Community Plan (OCP), there is an opportunity to establish a more coherent building height framework. This framework can promote seamless transitions between different zones, safeguard important view corridors, support growth objectives, and expand affordable housing options.

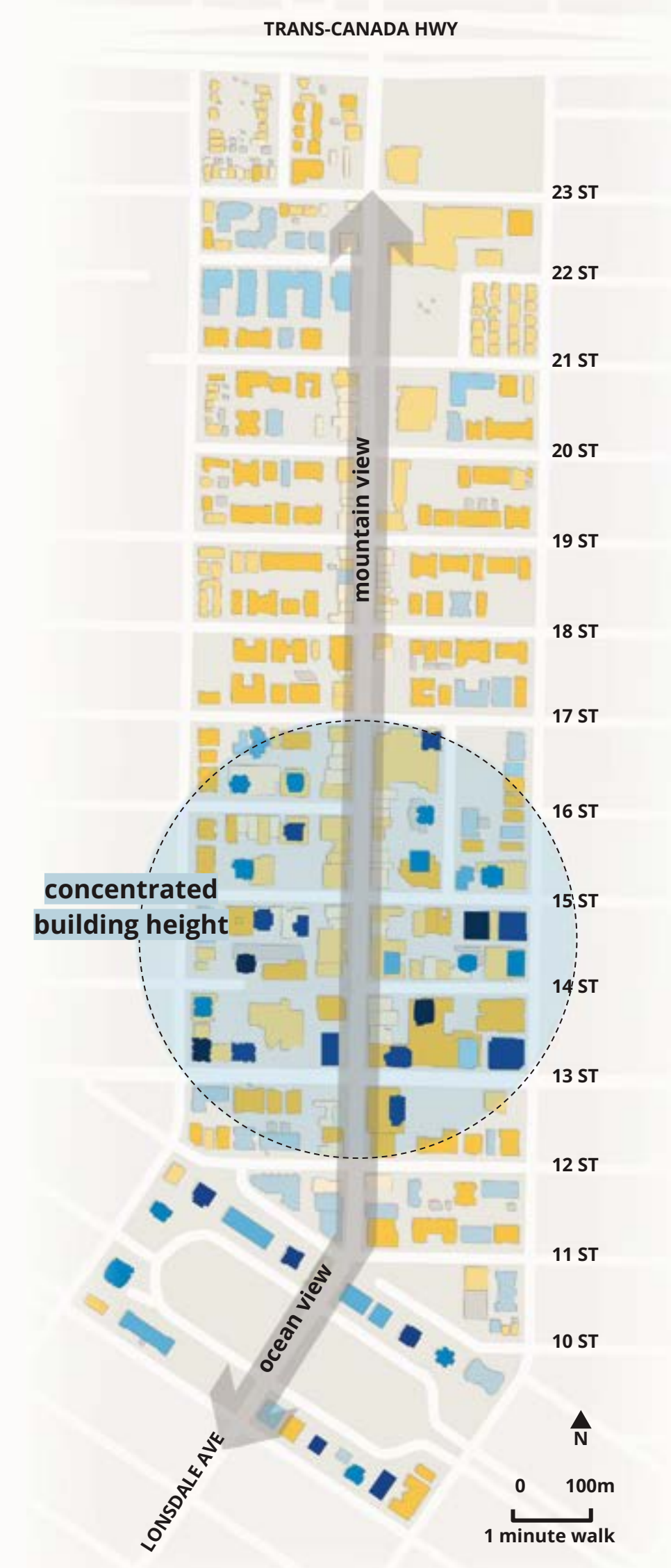
What we saw:



Corridor frames mountain views



Concentrated building heights within the urban core



What we heard:

“Please please keep the view to the mountains open.”

— 2025 Lonsdale Great Street Survey

“Restrict building heights along Lonsdale so it doesn't become a sun / light starved canyon.”

— 2025 Lonsdale Great Street Survey

Opportunities:

- **Graduated height transitions:** Graduated building heights, stepping down from the Urban Core to surrounding neighbourhoods, can create a more harmonious and visually interesting skyline.
- **View corridor protection:** Establishing precise height limits, informed by thorough view analysis, is essential to protect key vistas and preserve valued public views.
- **Daylight and sunlight access:** Careful consideration of building heights is necessary to ensure adequate daylight and sunlight access for both public spaces and adjacent properties, contributing to a comfortable pedestrian environment.
- **Contextual height limits:** Height limits tailored to the specific context of each area within Lonsdale, taking into account factors like street width, existing building heights, and proximity to transit and other amenities, will result in a more integrated and cohesive urban fabric.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important

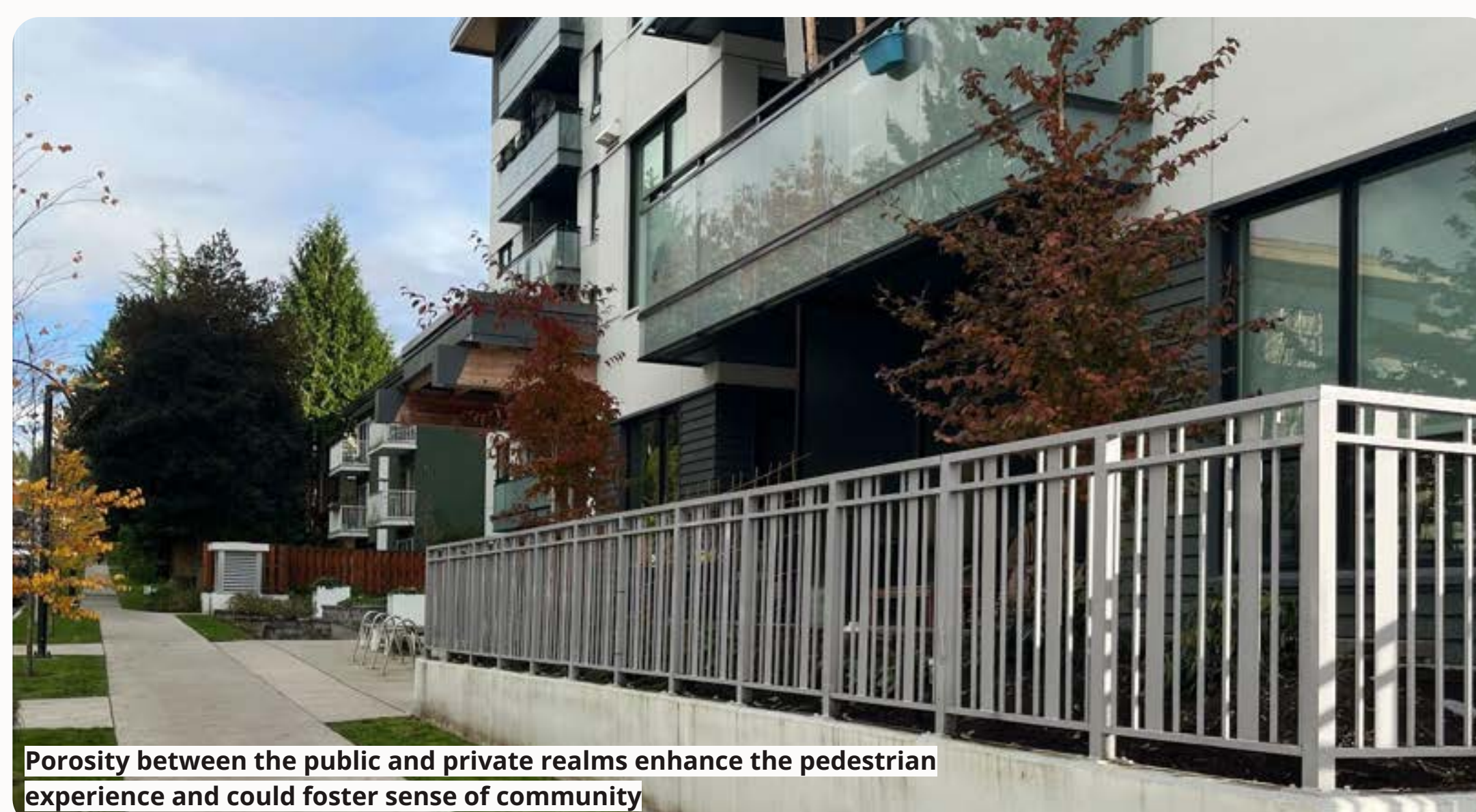
Very important



Beyond the facade: What makes a building engaging?

Central Lonsdale's mix of existing buildings and potential for future growth presents an opportunity to improve how buildings interact with the street, particularly at ground level. While the variety in building design adds to the neighbourhood character, inconsistent design approaches can lead to a disjointed experience at eye-level. Careful attention to edge conditions can improve liveability, support local businesses and create a more walkable community. Residential edges should balance privacy with street connection, to both provide a sense of safety and contribute to street life, while commercial edges should activate the street with engaging ground floor uses. Establishing clear guidelines will help create a more cohesive streetscape that strengthens the vital relationship between buildings and the public realm while allowing for architectural diversity.

What we saw:



Porosity between the public and private realms enhance the pedestrian experience and could foster sense of community



Silver Harbour Centre uses a change in height and varied native plantings to provide privacy



Ornamental landscape setbacks don't support local ecology

Opportunities:

- **Active frontage build-to lines:** Build-to lines, combined with active ground-floor uses, create more engaging and pedestrian-friendly street edges.
- **Pedestrian-scale articulation:** Upper-story setbacks and other articulation strategies contribute to human-scaled street walls, enhancing pedestrian comfort and access to light.
- **Integrated corner plazas:** Corner plazas, designed as parts of new developments, provide valuable public space and improve pedestrian flow at key intersections.
- **Building bulk management:** Thoughtful management of building bulk and scale transitions, particularly in areas of varied density, contributes to a more harmonious and visually appealing streetscape.
- **Architectural diversity within a framework:** Encouraging architectural diversity within a framework of cohesive design guidelines allows for individual expression while ensuring a harmonious streetscape character.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important

Very important



LAND USE & PLANNING

Living & Working in Central Lonsdale



Shaping the Northern Gateway: What's the right mix?

The Northern Gateway has the potential to become a vibrant mixed-use community anchored by the new recreation centre, Centennial Theatre, and Gordon Smith Gallery. However, this potential is hampered by several factors. The area includes mostly low-rise residential buildings, single-use commercial sites, and older buildings reliant on surface parking. This existing mix of uses lacks dedicated mixed-use commercial development. The OCP allows for building heights up to 30 storeys on the Harry Jerome lands—the highest in Central Lonsdale. This potential density requires careful consideration to ensure it contributes to a positive public realm and respects mountain views. Furthermore, the area faces challenges with underutilized parks, freeway noise, and a fragmented development pattern due to reliance on CD zones.

What we saw:

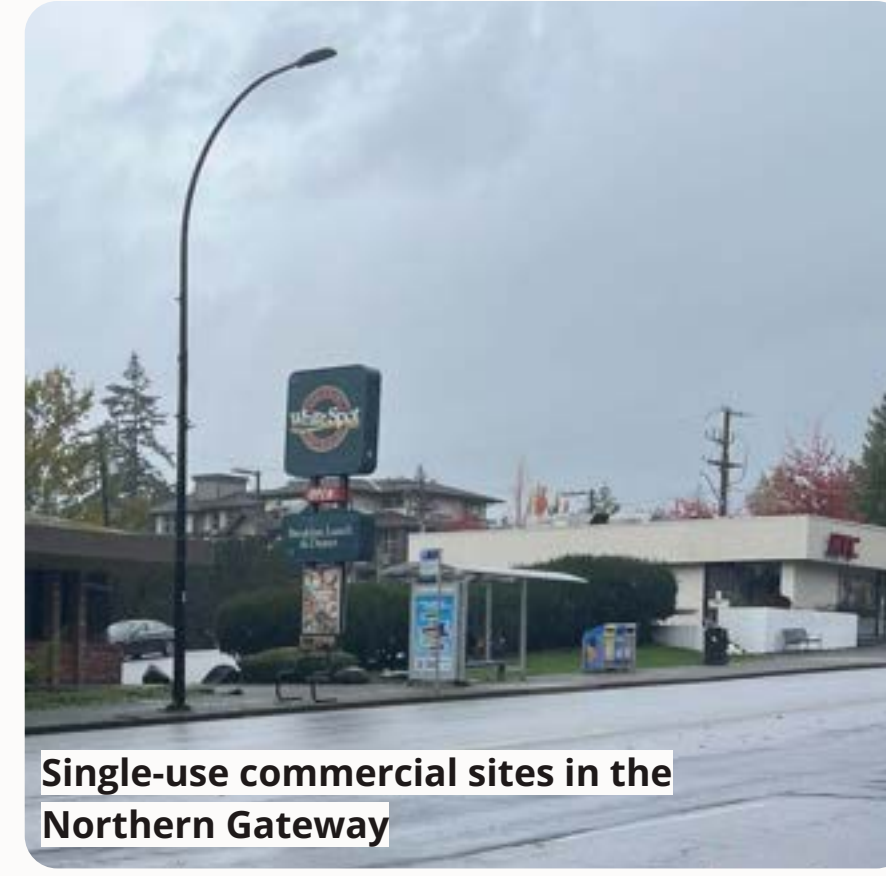
Housing oriented around important recreational and cultural uses, with large setbacks and greenspace.



The new Harry Jerome Recreation Centre will be a cultural anchor at the North end of Lonsdale Avenue



Low-rise residential makes up much of the Northern Gateway



Single-use commercial sites in the Northern Gateway



Opportunities:

- **Cohesive form and character:** A comprehensive set of form and character guidelines, rather than individual CD zones, can guide redevelopment, ensuring a harmonious urban fabric and mitigating the risk of disjointed architectural expression.
- **Integrated cultural and recreational amenities:** New development has the opportunity to integrate with existing cultural and recreational amenities, such as the new recreation centre, Centennial Theatre, and Gordon Smith Gallery, creating a vibrant cultural hub.
- **Optimized density and height:** Revising the density and building height framework, potentially allowing for greater density and building heights near key amenities, and focusing on infill development at sites with existing car-oriented uses, can support a more vibrant mixed-use community.
- **Fine-grained commercial landscape:** Supporting finer-grain commercial retail units (CRUs) in new development can provide more pedestrian-focused activation throughout the day, catering to the needs of residents, visitors, and cultural venues.
- **Diverse mix of uses:** Encouraging a diverse mix of uses, including residential, commercial, and community spaces, creates a more vibrant and resilient neighbourhood, providing residents with easy access to a range of amenities and services.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important



Very important

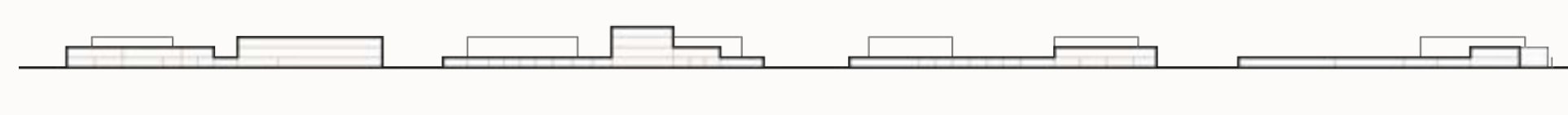


The Village: How do we create a more walkable neighbourhood?

The Village has a unique character with its mix of stores and homes, but is challenged in creating a truly pedestrian-friendly environment. Some large sites disrupt the pedestrian realm with large surface parking lots. Older buildings reliant on surface parking add to this challenge. Current zoning (C-2) allows for a mix of uses, but the permitted floor area restricts building heights and scale of development. Additionally, small lots further hinder redevelopment. This limits the potential for a more vibrant mix of uses to be added to this area, as envisioned by the Official Community Plan (OCP). Redevelopment of the two large sites presents an opportunity to create a more cohesive public realm. To be successful, this could consider underground parking, anchor tenants, and laneway servicing to support both existing and new businesses.

What we saw:

Low-density, single-use commercial surrounded by mid-rise housing with large setbacks, parking lots and big box commercial uses.



Fine-grained retail contributes to intimate street feel



Fine-grained retail with porous facades activates the sidewalk



Strip mall surface parking lots detract from the pedestrian experience



Surface parking lots deactivate the sidewalk



Opportunities:

- **Optimize density and building form:** The current C-2 zoning could be revised to encourage greater building heights and a more diverse mix of building forms, contributing to a more defined and active street edge.
- **Transform key redevelopment sites:** The future redevelopment of specific sites offers a significant opportunity to enhance the public realm and provide valuable community amenities.
- **Incentivize infill development:** Density bonusing provisions could be strengthened to encourage infill development up to 6-storeys, or even 8-storeys, subject to tenure and community amenity considerations.
- **Address parking and servicing:** Future redevelopment could carefully consider parking and laneway servicing needs, exploring innovative solutions like shared parking facilities, promoting alternative transportation options, and optimizing laneway design to support both residential and commercial uses.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important



Very important



LAND USE & PLANNING

Living & Working in Central Lonsdale

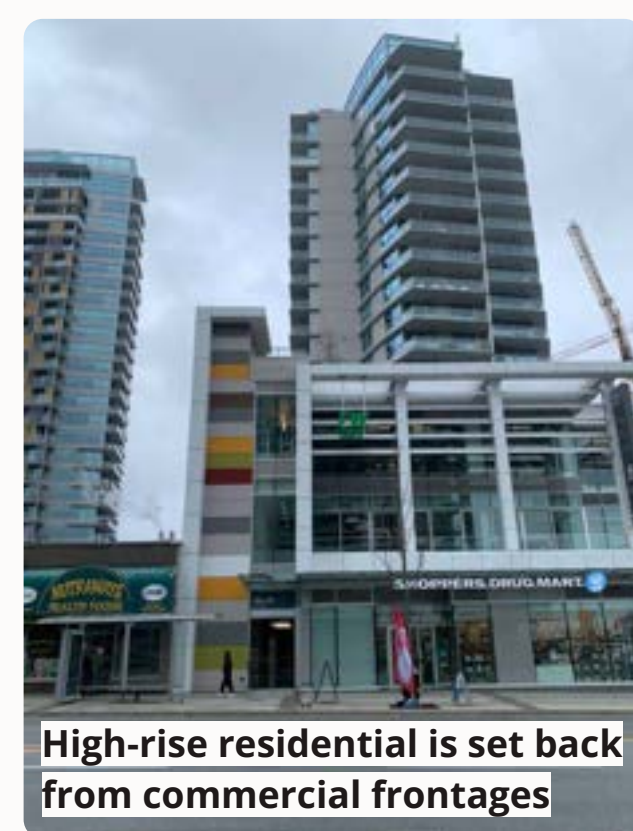


Urban Core: How do we balance density with a vibrant public realm?

The Urban Core is the most intensely developed part of Central Lonsdale, with a mix of high-rise residential, commercial, and institutional uses, as well as a significant concentration of fine-grain retail. However, the existing land use framework, with its mix of zones and reliance on Comprehensive Development (CD) zones, can lead to inconsistencies in building heights and setbacks and as a result, a varied pedestrian experience. A more unified approach, aligned with the Official Community Plan (OCP) designations could ensure a more cohesive and human-centred environment that balances density with a vibrant public realm. Additionally, Chesterfield and St. Georges Avenues could better support commercial activity and building height transitions. Exploring the potential for road closures in certain areas could further enhance street activation in this high-density part of Central Lonsdale.

What we saw:

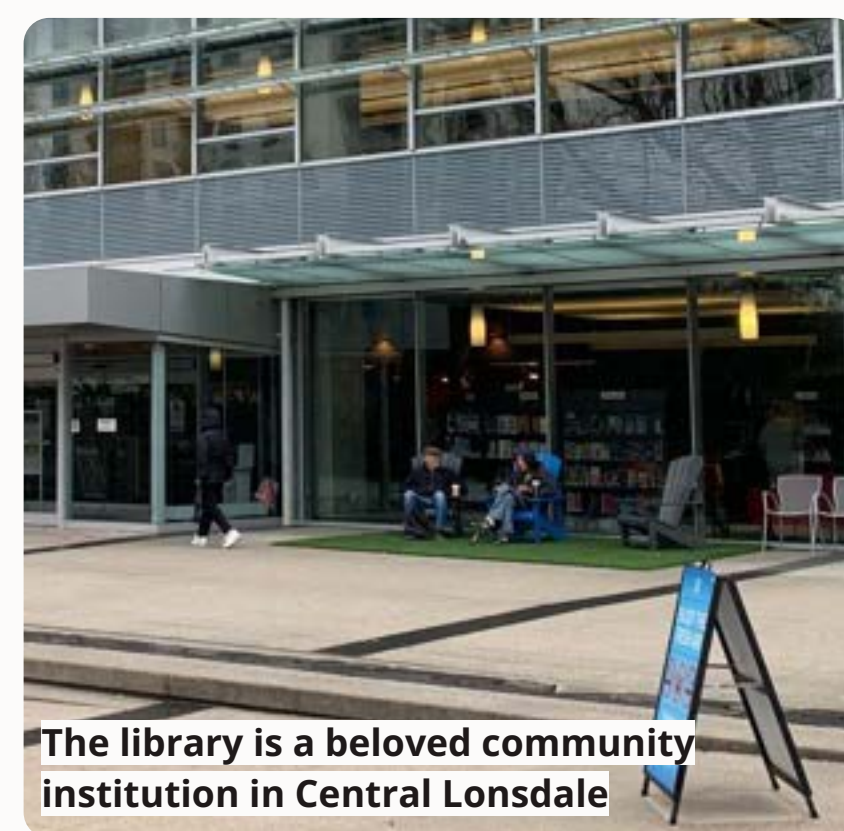
Higher density mixed-use supported by larger anchor commercial tenants with fine grain businesses mixed in.



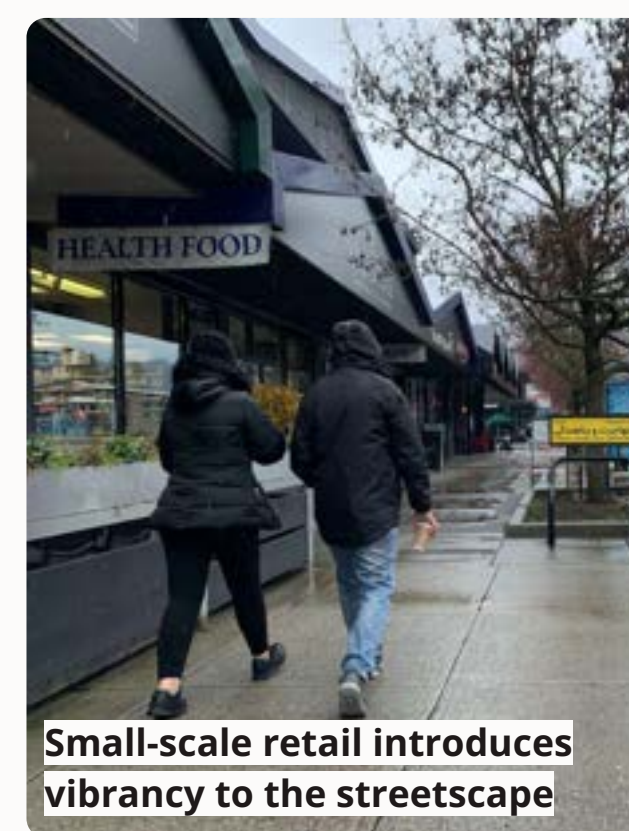
High-rise residential is set back from commercial frontages



City Market, an anchor commercial tenant



The library is a beloved community institution in Central Lonsdale



Small-scale retail introduces vibrancy to the streetscape



Opportunities:

- **Optimized density and height for housing diversity:** Reviewing the height framework to support greater dwelling unit flexibility and exploring density/height bonusing could incentivize a wider range of housing options.
- **Incentivize mixed-use development along Lonsdale:** Incentivizing mixed-use projects along Lonsdale Avenue could create a more vibrant and active streetscape, drawing people in and activating the area.
- **Form and character guidelines with a focus on daylight and space quality:** New Form and Character DP Guidelines, with a strong focus on daylight/sunlight impacts and the quality of amenity spaces, could ensure that development contributes positively to the public realm.
- **Laneway activations and road closures:** Reimagining the laneway network as pedestrian-friendly spaces and consider strategic road closures in higher-density areas to expand the public realm and create pedestrian-only zones to provide opportunities for community events and activities, enhancing the social and cultural vitality of the area.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important



Very important



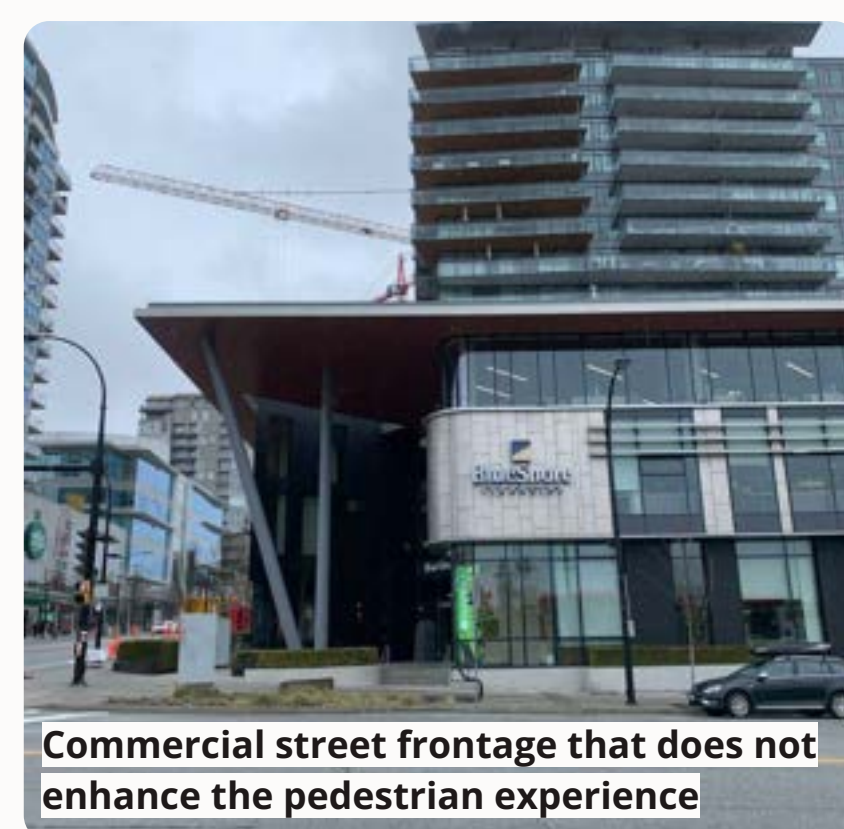
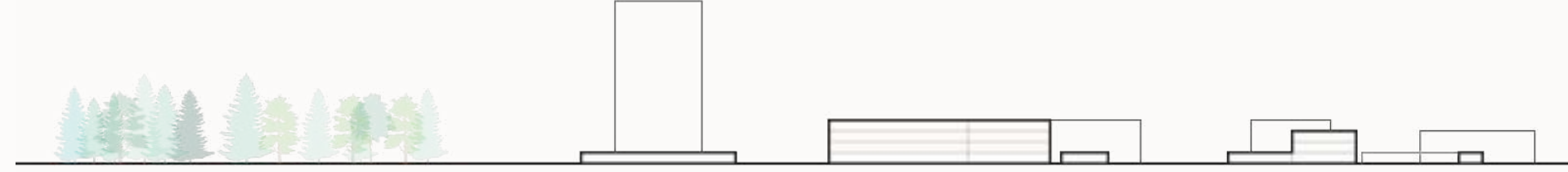
Southern Gateway: What kind of welcome do we want to create?

The Southern Gateway, anchored by Victoria Park, is the first impression for many visitors to Central Lonsdale. Spanning from 10th Street to 13th Street, the Southern Gateway contains a mix of large-scale commercial and mixed-use developments surrounded by residential buildings.

Currently, this area suffers from intermittent ground level retail which reduces commercial vibrancy and pedestrian activity. Reimagining the role of Victoria Park, introducing active street frontages, and strengthening connections to the Urban Core provides an opportunity to create a vibrant and welcoming arrival experience that sets the stage for a memorable visit to Central Lonsdale.

What we saw:

Low-density, single-use commercial surrounded by mid-rise housing with large setbacks, parking lots and big box commercial uses.



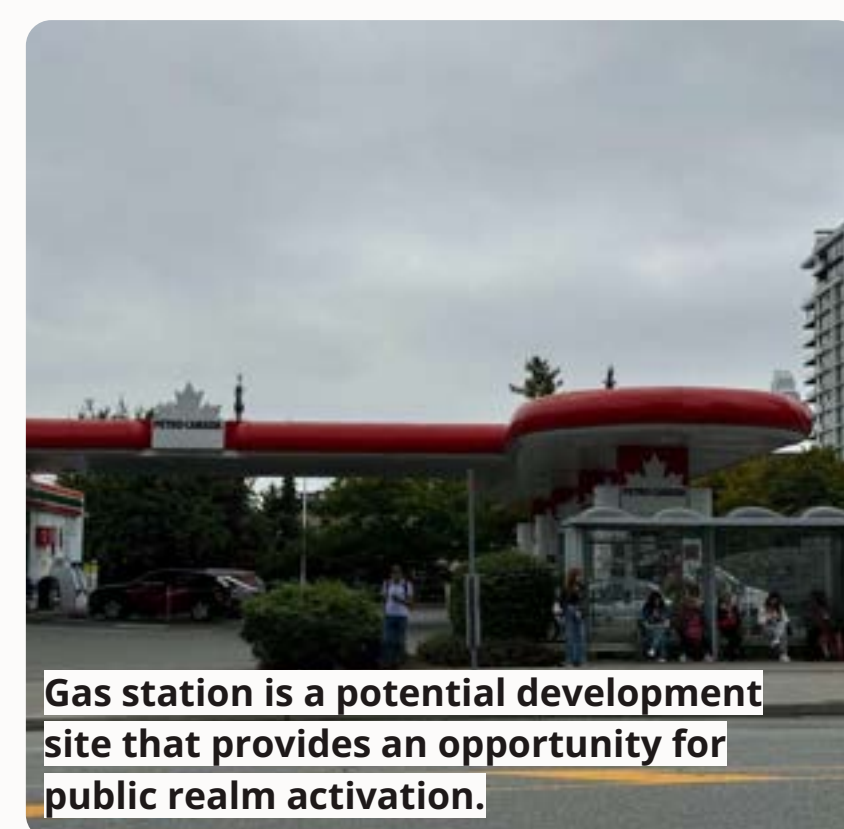
Commercial street frontage that does not enhance the pedestrian experience



Mixed-use building with porous facade and public art activates the sidewalk



Opaque facades take away from the pedestrian experience



Gas station is a potential development site that provides an opportunity for public realm activation.



Opportunities:

- **Catalyze redevelopment:** A new density and height framework could support the redevelopment of older low-to-mid-rise buildings, incorporating underground parking to alleviate on-street pressures.
- **Activate key intersections:** Redevelopment of key corner sites at Lonsdale Avenue and 13th and 12th Streets offers a prime opportunity to improve public realm activations.
- **Enhance commercial frontage:** Strategies to enhance commercial frontages could include new development, storefront improvements, and attracting a diverse mix of businesses.
- **Strengthen pedestrian connections:** Prioritizing pedestrian connectivity through a safe and inviting pedestrian realm can better link the Southern Gateway to the rest of Central Lonsdale.
- **Activate Victoria Park:** Increasing programming in Victoria Park could make it a more vibrant destination.
- **Create a sense of arrival:** Thoughtful design can signal arrival to Central Lonsdale by incorporating public art, signage & wayfinding that reflect the neighbourhood identity.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



Not important



Very important



ECONOMIC ASSESSMENT

Working and Doing Business in Central Lonsdale

Central Lonsdale Businesses: How can we ensure continued growth and success?

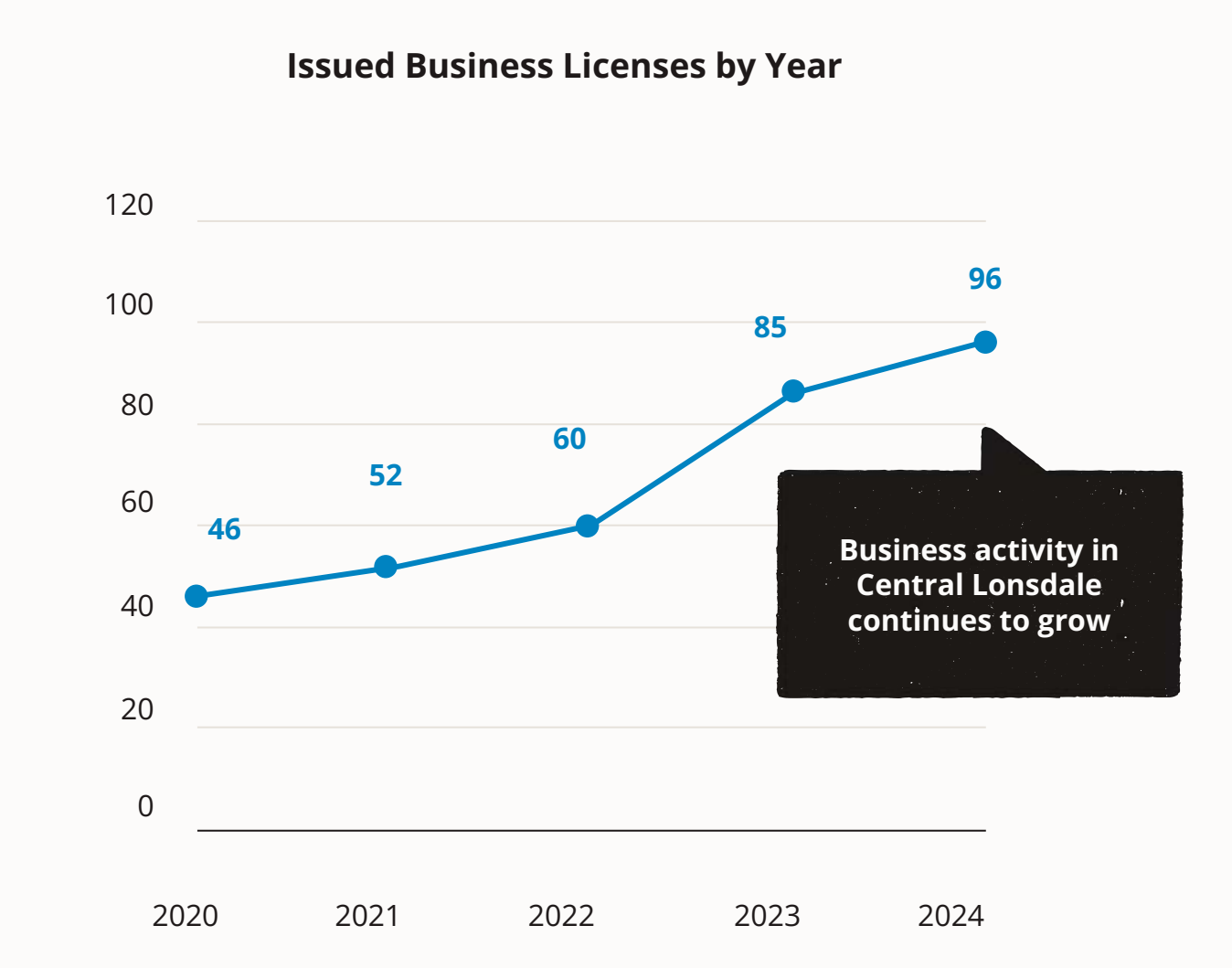
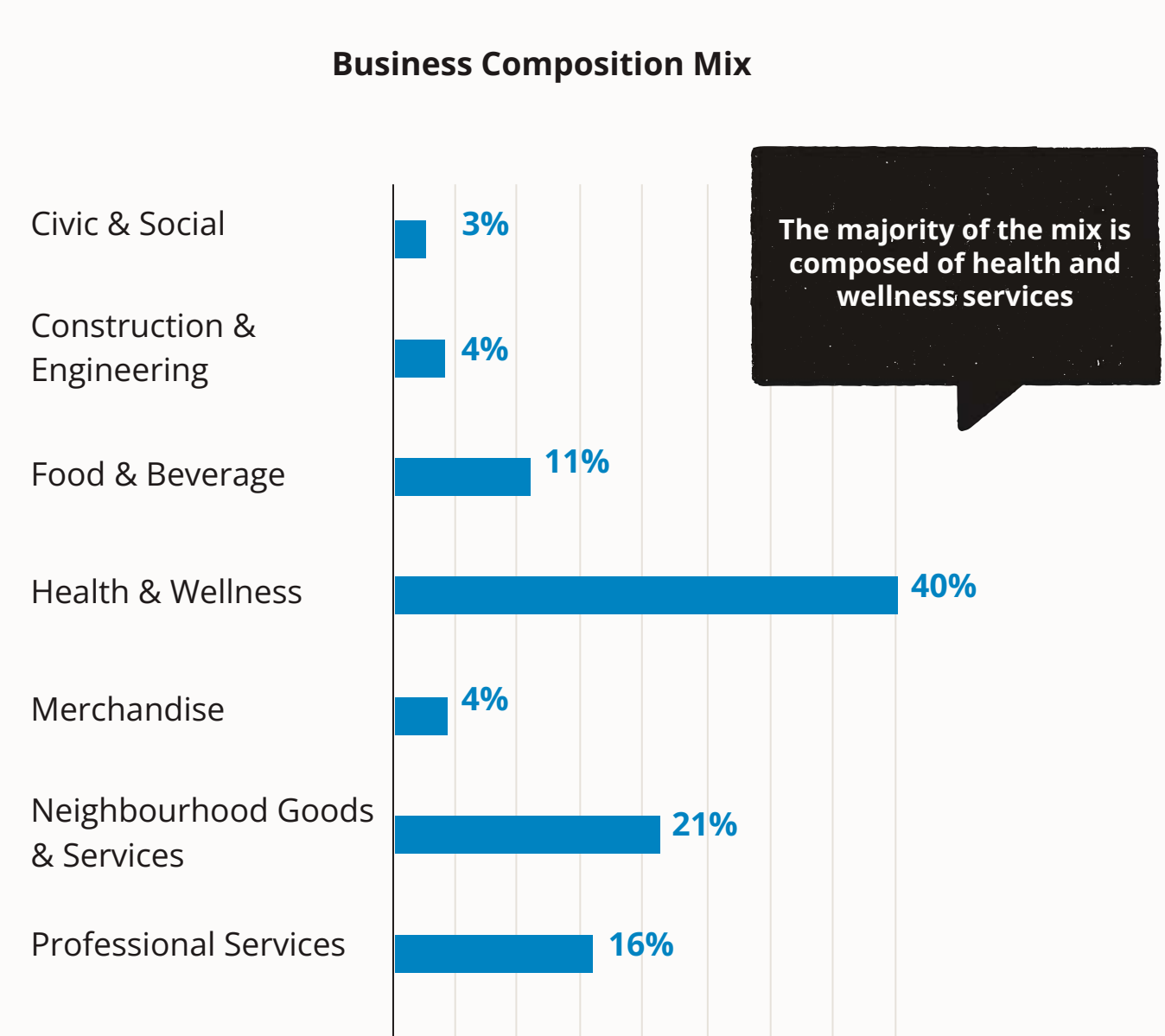
Central Lonsdale boasts a diverse and growing business community, with approximately 500 businesses currently operating in the area. Nearly 100 new businesses opened in 2024, exceeding the number of openings in 2020. This growth is largely driven by the health and wellness sector.

Interestingly, while health and wellness businesses are the largest business type in the area, our survey revealed that most people visit Central Lonsdale to dine. This highlights the diverse appeal of the area.

Despite a slight dip in retail sales in 2024, overall sales grew by 3.3%, demonstrating the resilience of the local economy. This growth is attributed to strong spending in sectors like professional and medical services, even outperforming Downtown Vancouver.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:



Licensing data includes all businesses located on Central Lonsdale, including storefront businesses and office building tenants. Source: City of North Vancouver

What we heard:

81% respondents typically visit Central Lonsdale to dine at restaurants and cafes.

76% respondents typically visit Central Lonsdale to run errands.

55% respondents typically visit Central Lonsdale for health and wellness services.

“It would be nice to have a greater variety of shops - a book shop, a stationery and gift shop, a local specialty coffee shop like Thomas Hass, a quaint wine bar...”
- 2025 Lonsdale Great Street Survey

Opportunities:

- ➔ **Diversify retail:** Attract a wider range of businesses, including food & beverage, arts & culture, and evening destinations.
- ➔ **360 amenities:** Provide all-round retail amenities to meet the needs of residents and visitors alike.
- ➔ **Facade guidelines:** Implement guidelines to ensure businesses activate the public realm.
- ➔ **Create destinations:** Augment neighbourhood-serving commerce with destination dining and retail that will attract visitors and residents alike.

Not important Very important

Central Lonsdale: How can we ensure new development enhances our community?

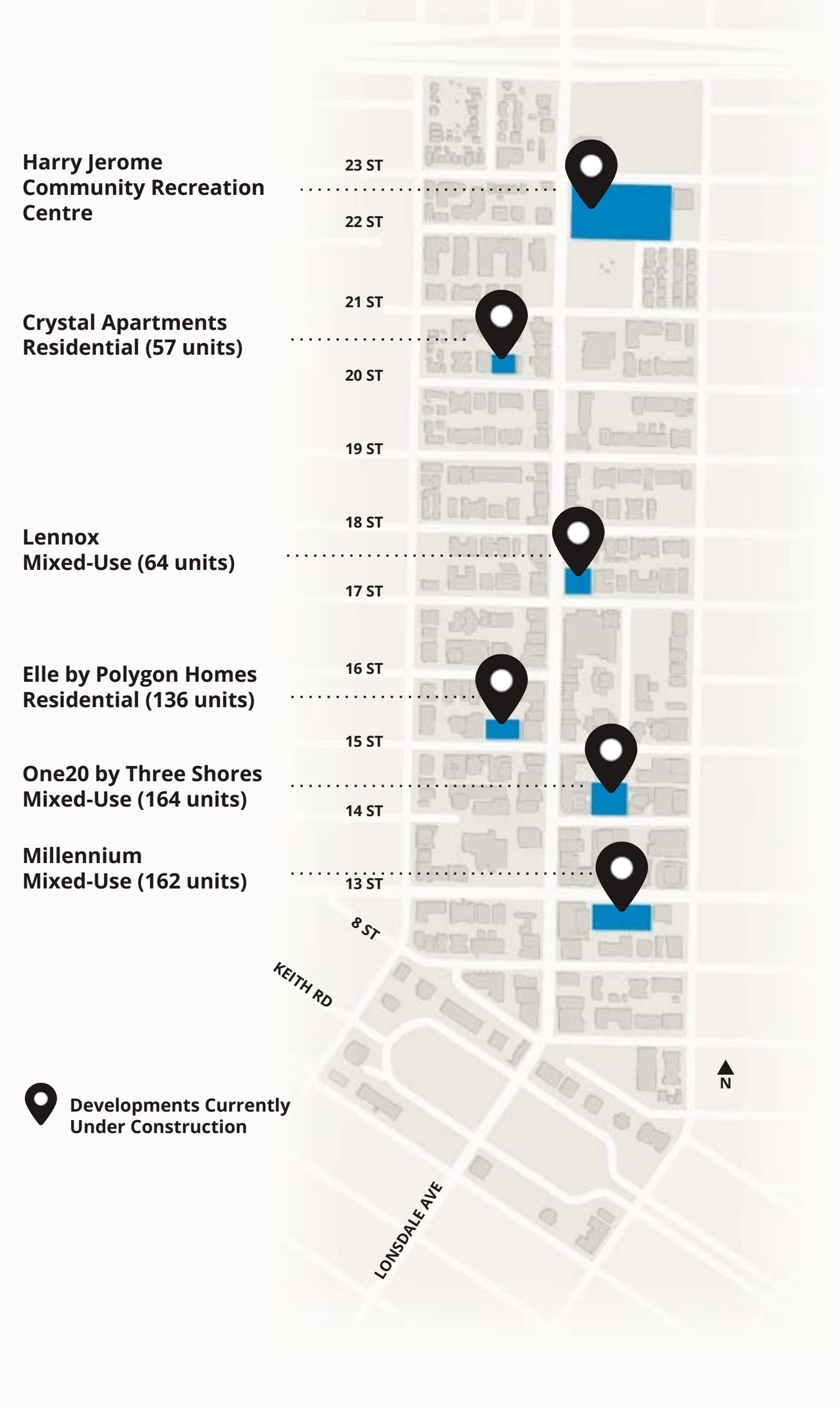
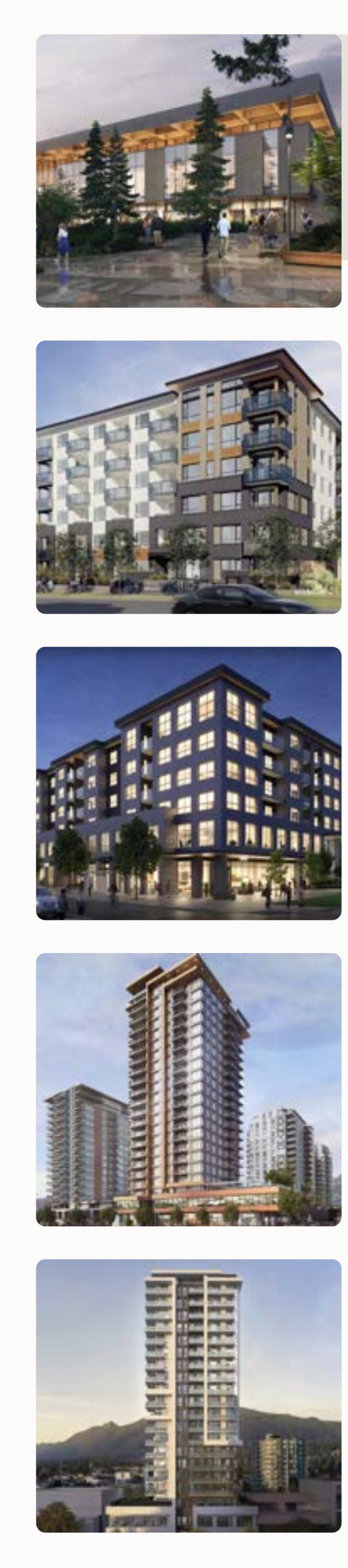
Central Lonsdale is experiencing a period of exciting growth and transformation, attracting new investment, residents, and businesses. This growth is fueled by strong market fundamentals and the area's appeal as a vibrant and amenity-rich community. Over the next decade, Central Lonsdale is anticipated to see a 20% increase in population, adding 4,500 new residents¹. This growth is evident in the numerous developments underway, including six new projects that will add 583 units of housing. These new residents will further support the existing businesses and create opportunities for new businesses to thrive.

As Central Lonsdale continues to evolve, it's important to ensure that new developments complement and enhance the character of the area.

¹ Central Lonsdale Situation Assessment (Colliers, 2023)

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:



What we heard:

“I wish there was more great housing, more condos and rental options.”
- 2025 Lonsdale Great Street Survey

“I hope Central Lonsdale can be a hub where it's less expensive to gather and create a community.”
- 2025 Lonsdale Great Street Survey

“Need more mixed used, low-rise development”
- 2025 Lonsdale Great Street Survey

Survey respondents would like to see a variety of amenities in Central Lonsdale:

| | | | |
|---------------------------------------|--------------------------------------|-----------------------------|------------------------------|
| 65% Local businesses & shops | 52% Food & beverage offerings | 34% Retail offerings | 33% Park / play areas |
| 30% Cultural / music offerings | 29% Community event spaces | 28% Daily amenities | |

Opportunities:

- ➔ **Character-based zoning:** Character-based zoning overlays guide development and reinforce area character. A building height framework can offer greater control over urban form than FSR (Floor Space Ratio).
- ➔ **OCP density bonuses:** Density bonuses tied to community benefits (affordable housing, art, sustainability) incentivize desired development aligned with OCP goals.
- ➔ **Street activation metrics:** Linking mixed-use incentives to performance metrics (active frontage, business types, plaza design) optimizes street activation.
- ➔ **Measurable pedestrian standards:** Design standards with measurable outcomes (sidewalks, trees, facades) enhance the pedestrian realm.
- ➔ **Integrated public realm:** A city-led public realm strategy, potentially a Master Plan, creates a cohesive network of public spaces.

Not important Very important



LAND USE, PLANNING AND ECONOMIC ASSESSMENT

Tell us your thoughts & ideas!

Q

When planning for Central Lonsdale's future, what's most important to protect or improve?

Q

What kind of shops, restaurants, services, and amenities are missing in Central Lonsdale that you'd like to see?

Q

Is there anything else you'd like to share with us?

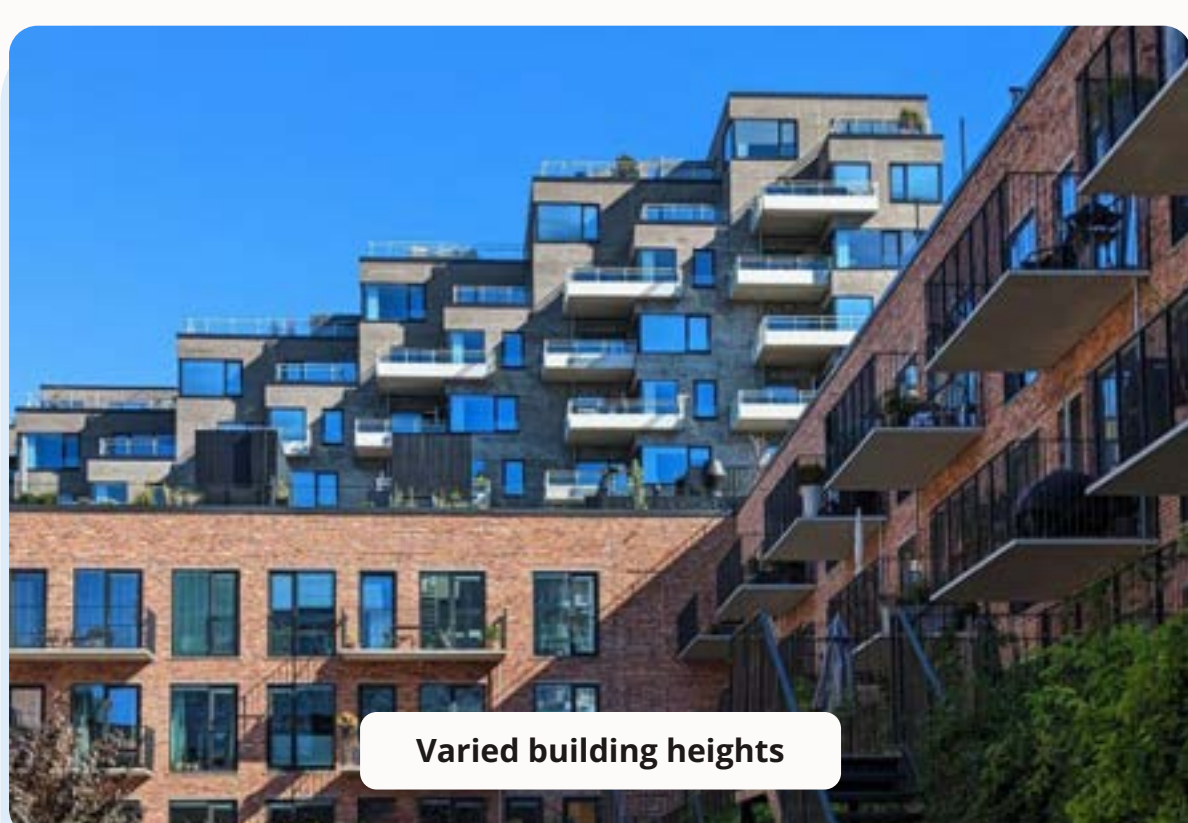
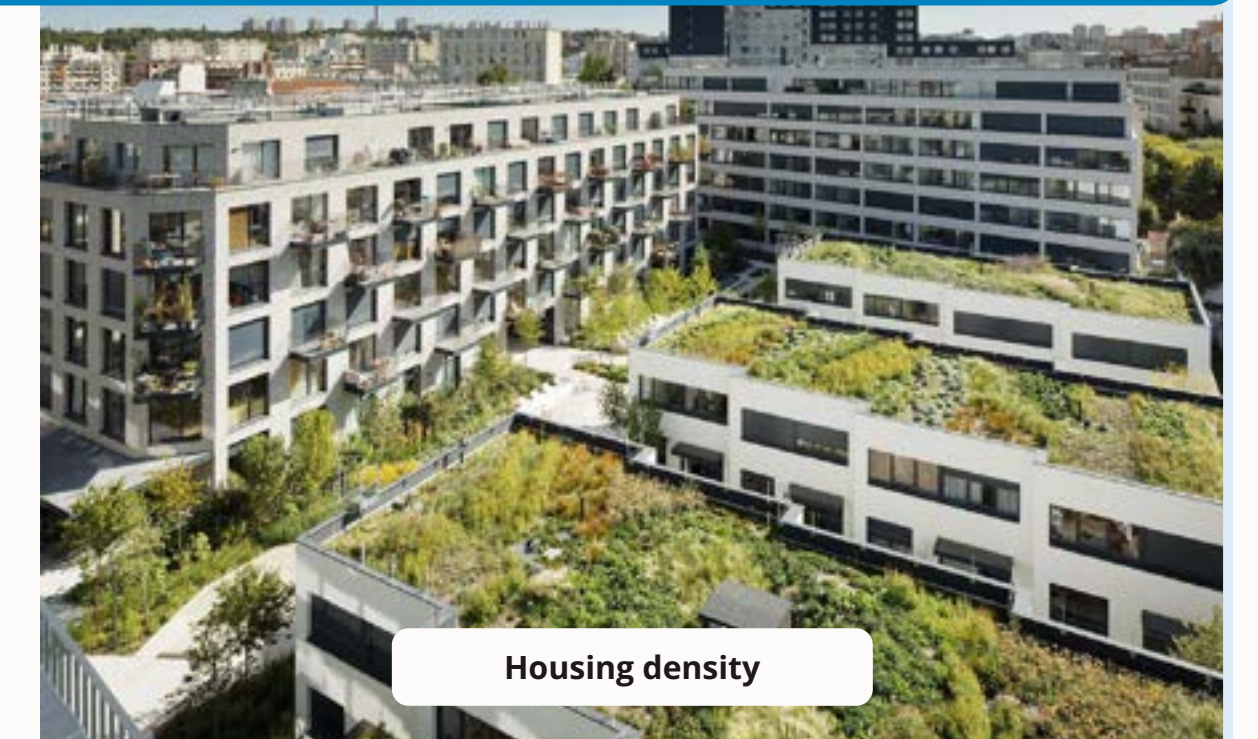
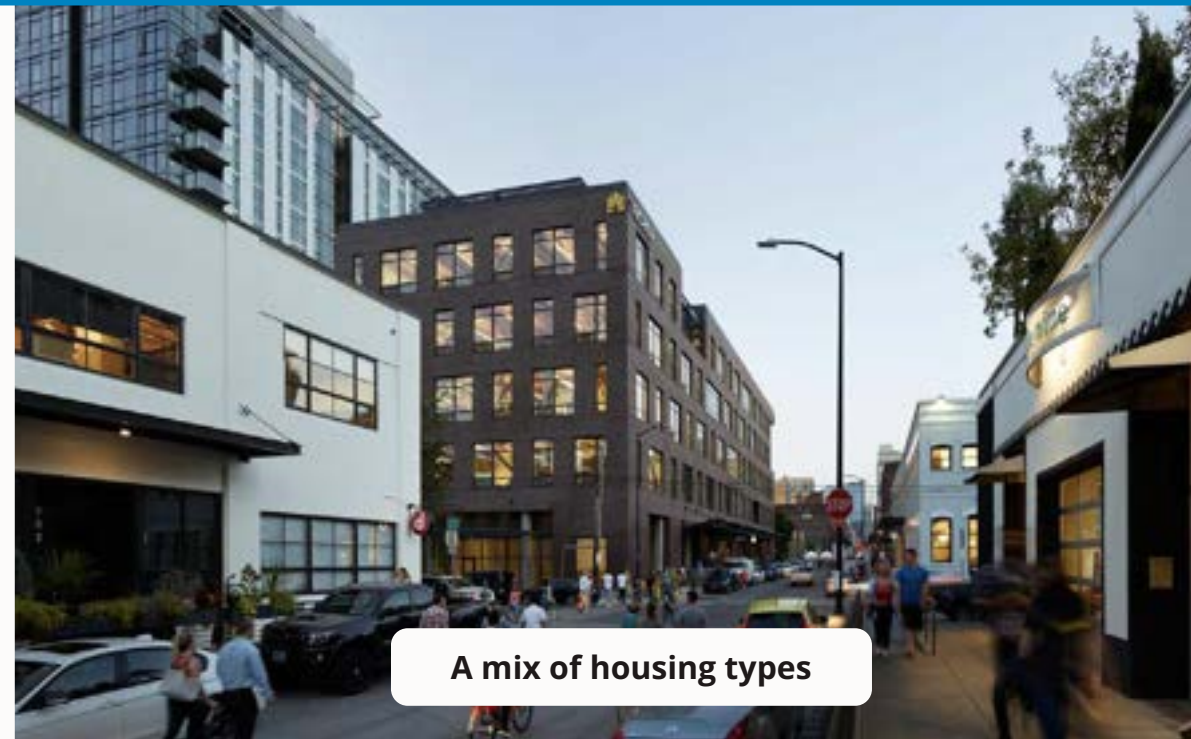
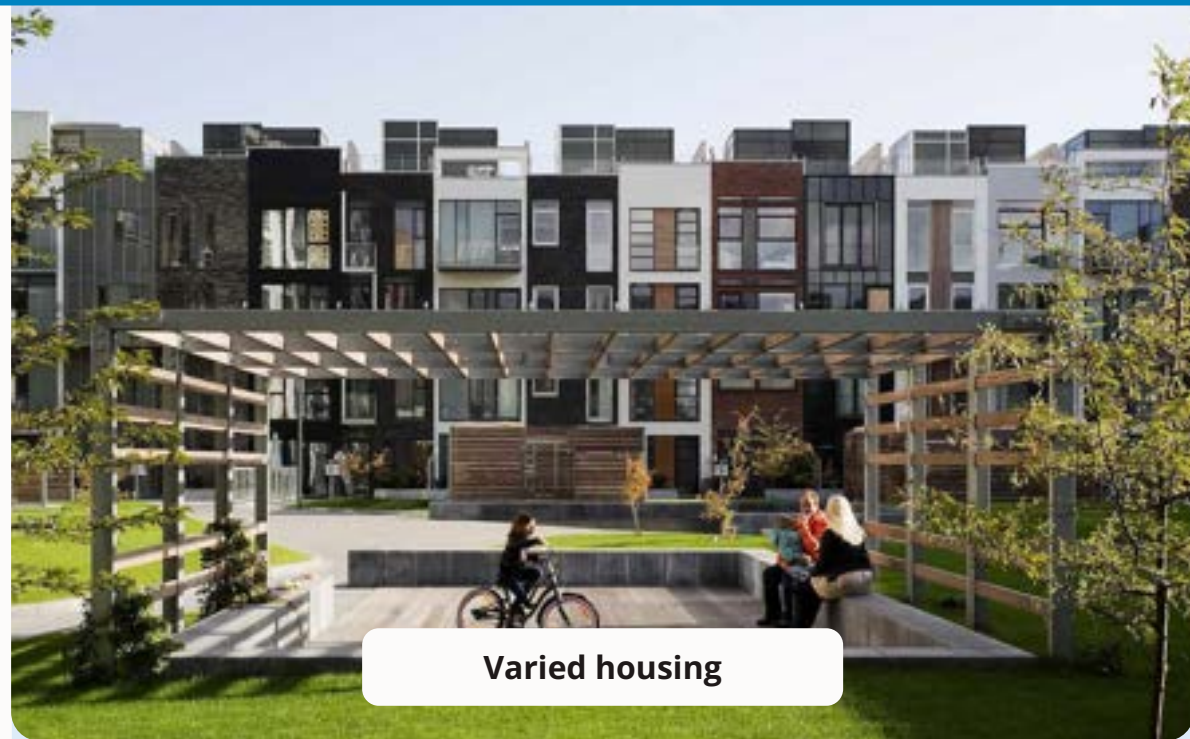
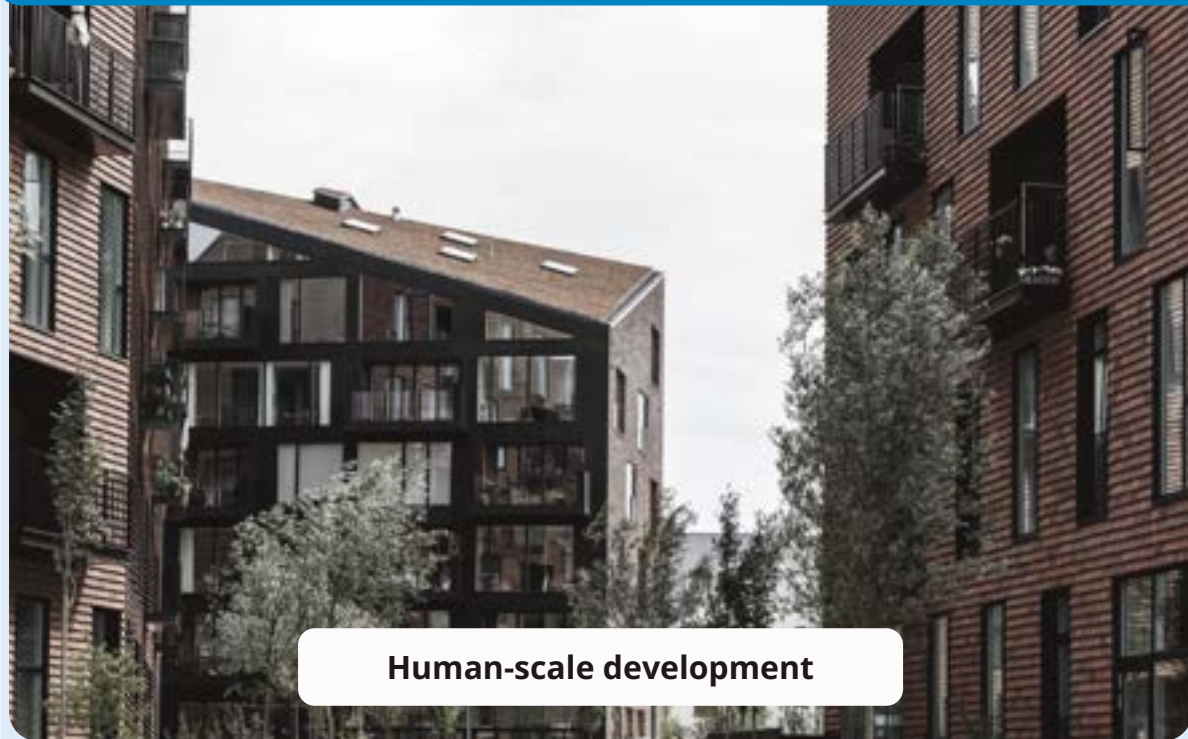




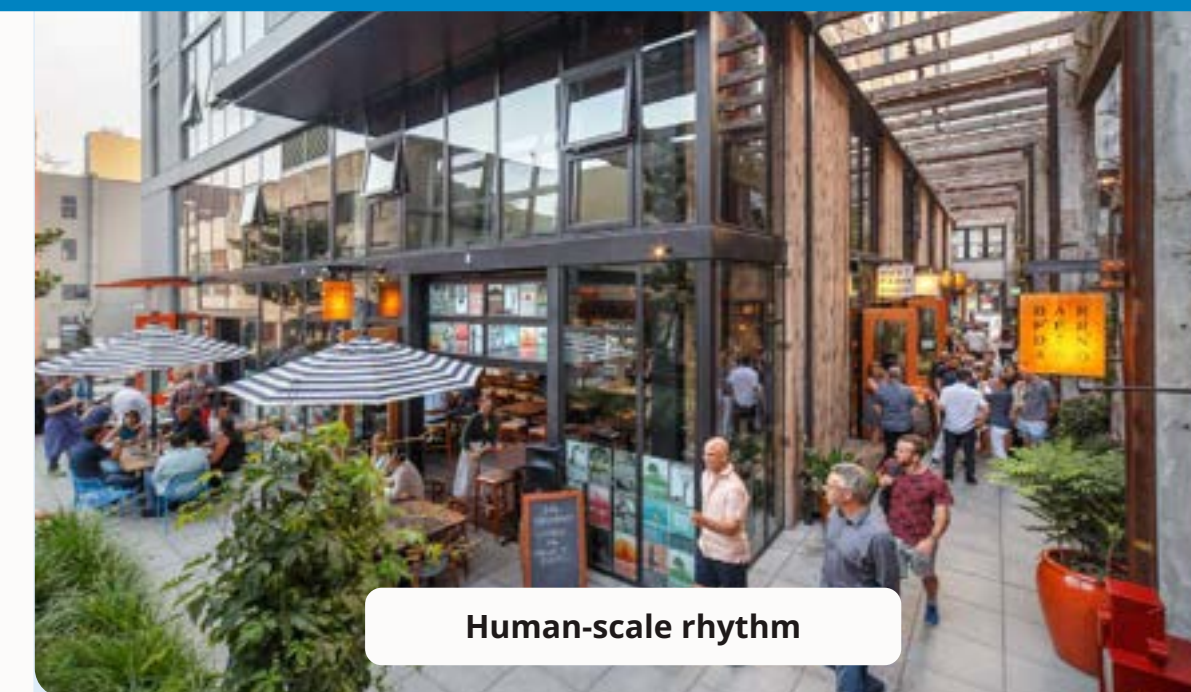
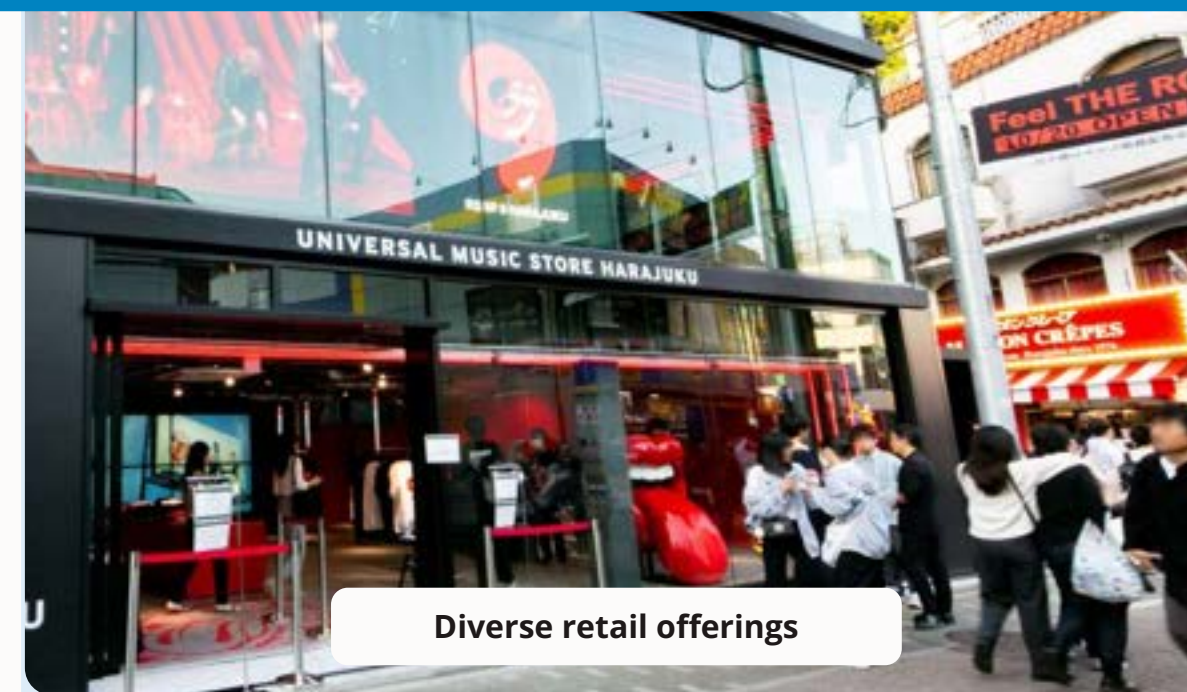
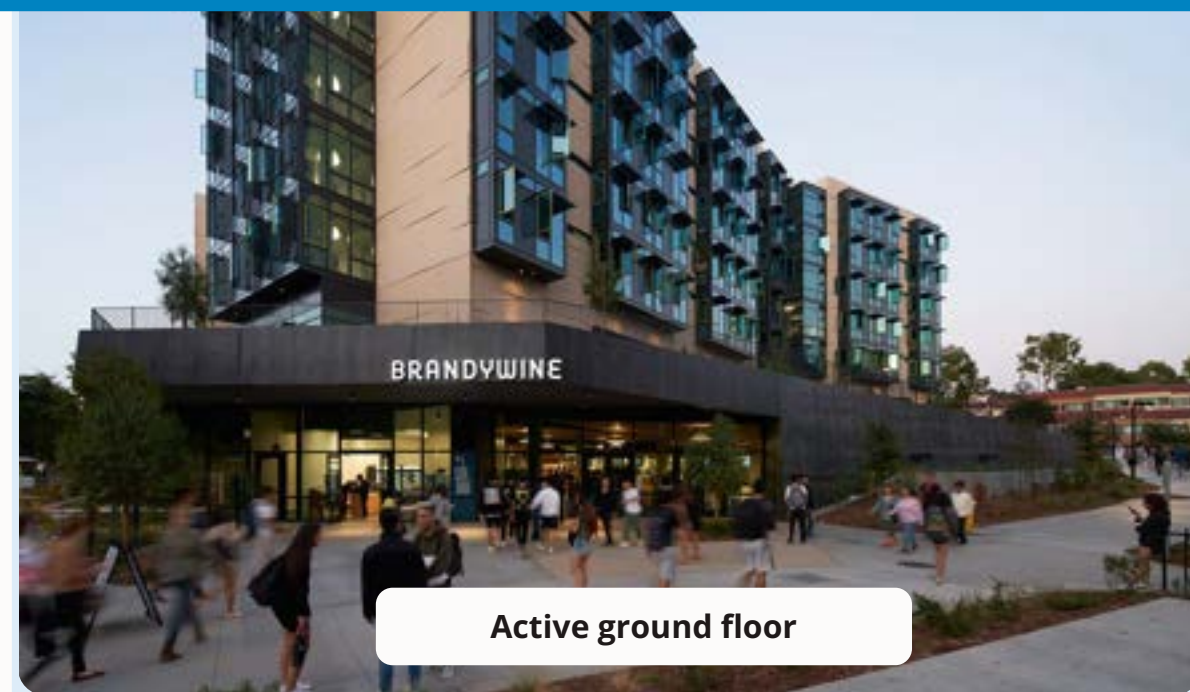
LAND USE, PLANNING AND ECONOMIC ASSESSMENT

Inspiration Images

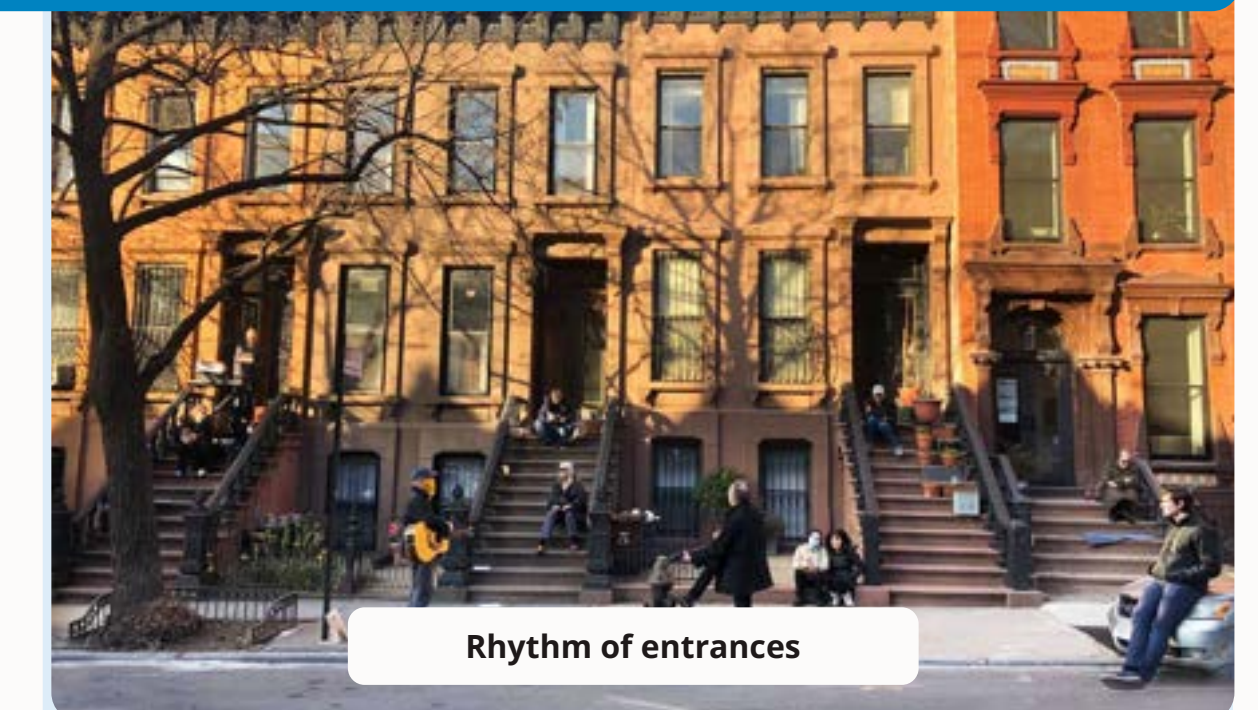
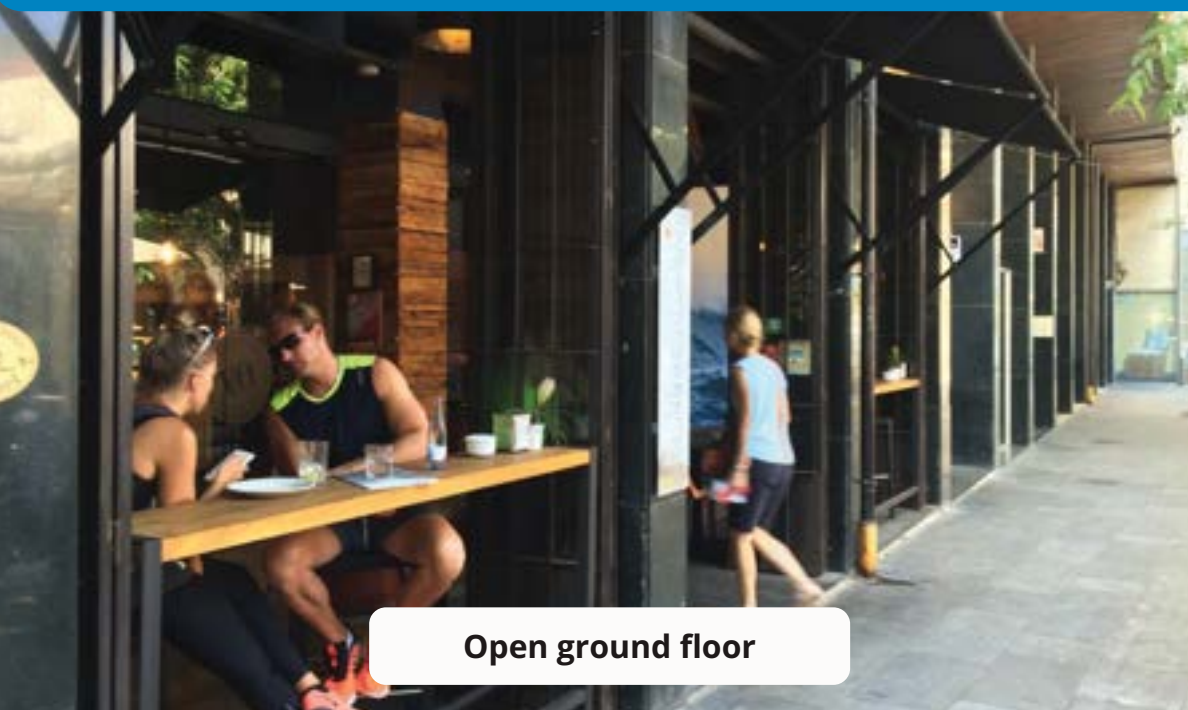
Building density and height



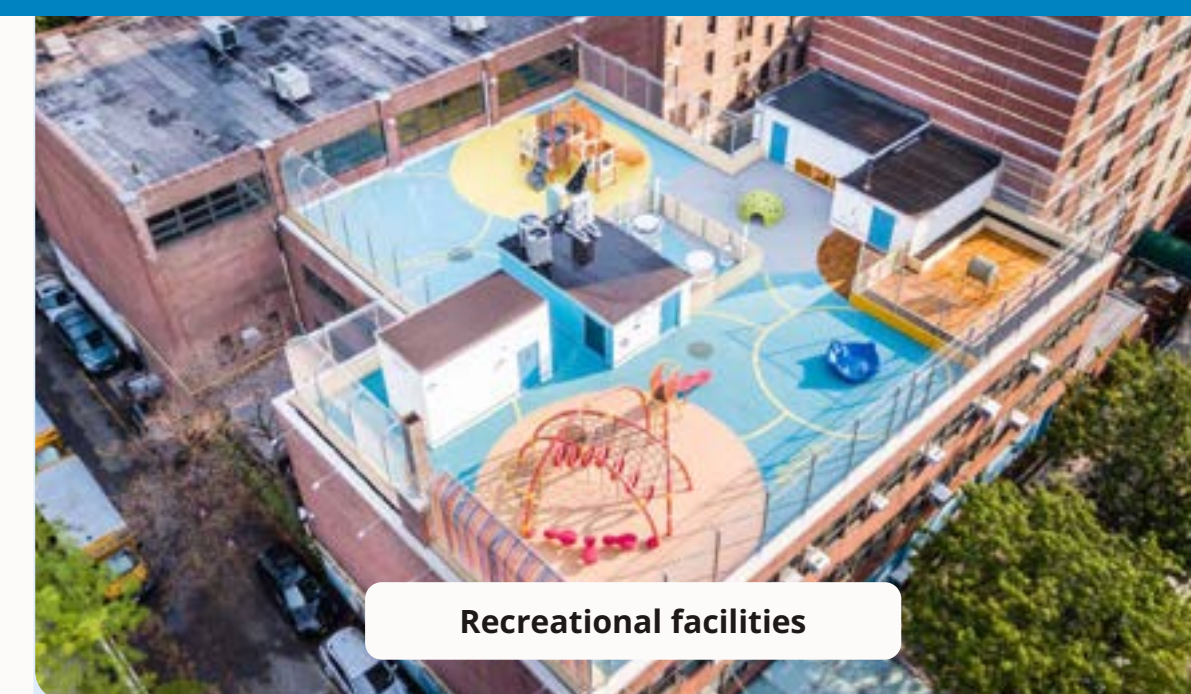
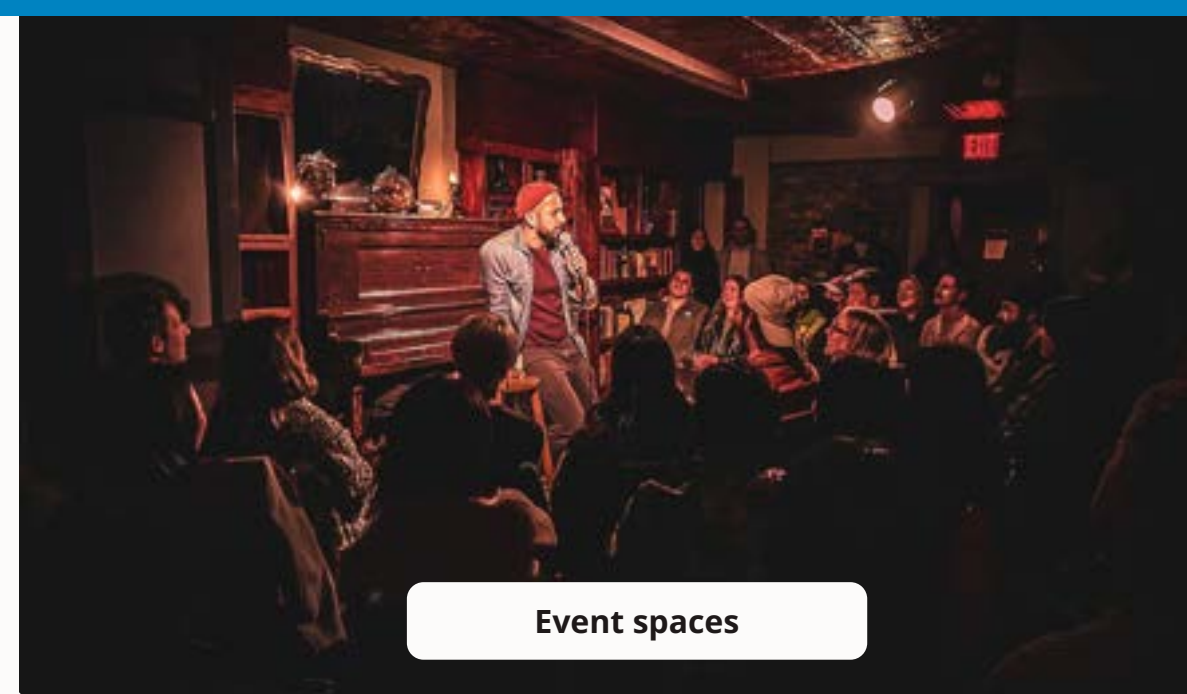
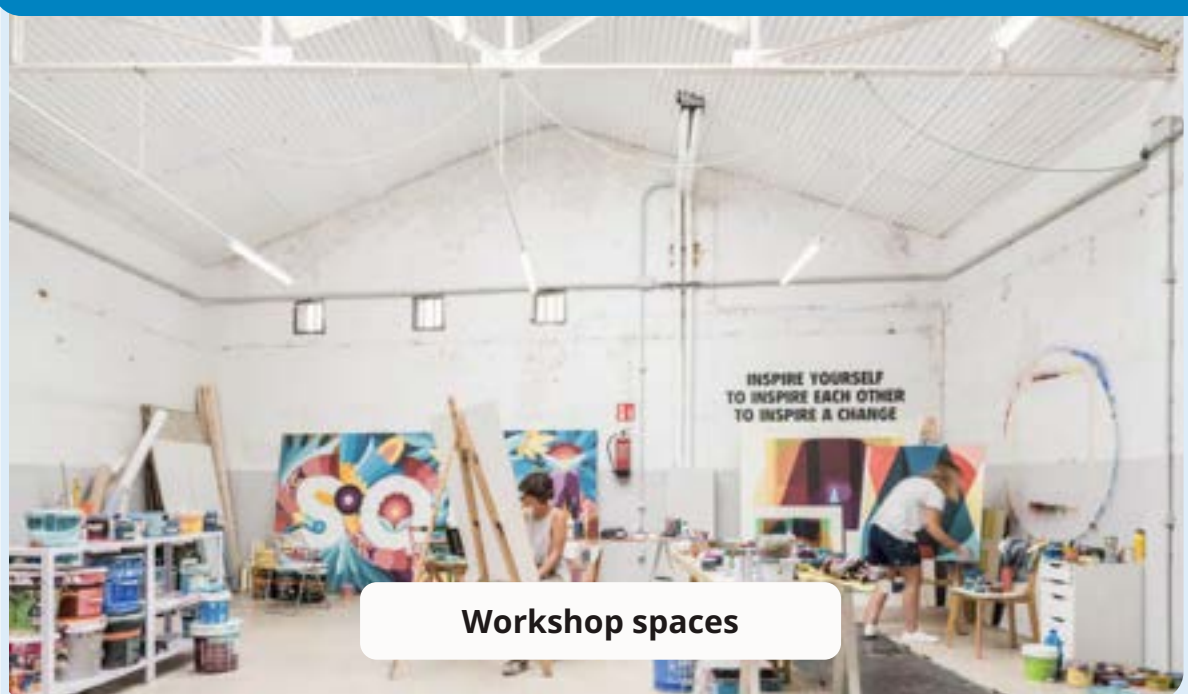
Stacked functions and a variety of functions



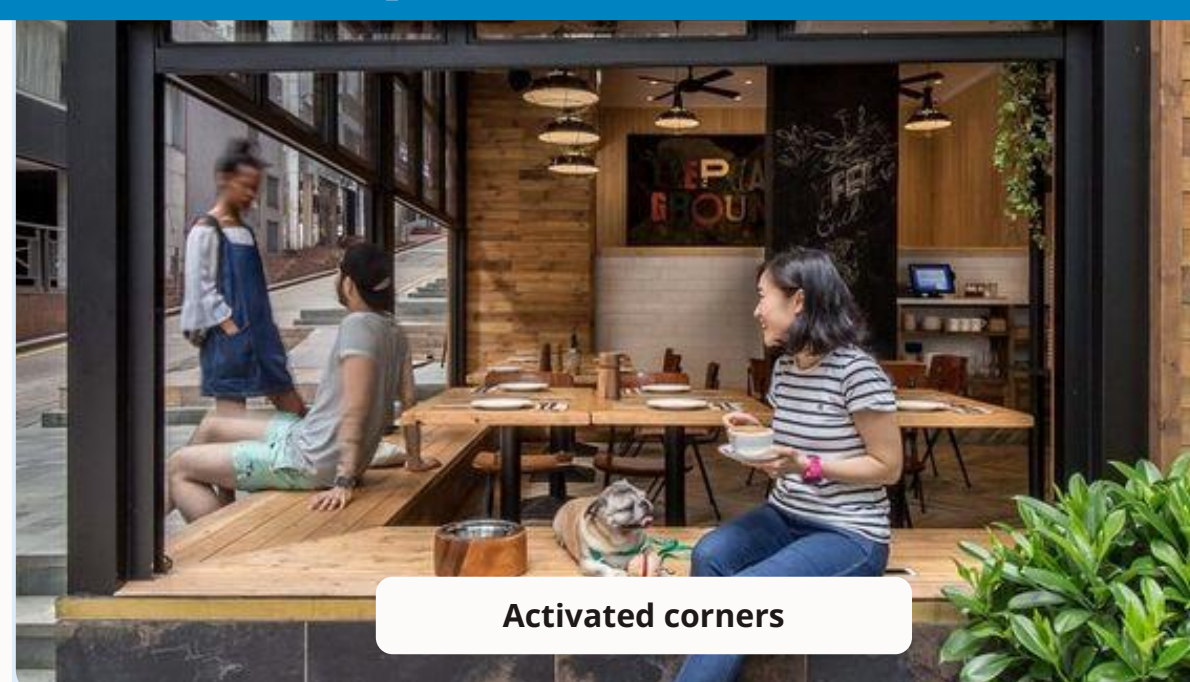
Engaging and active edges



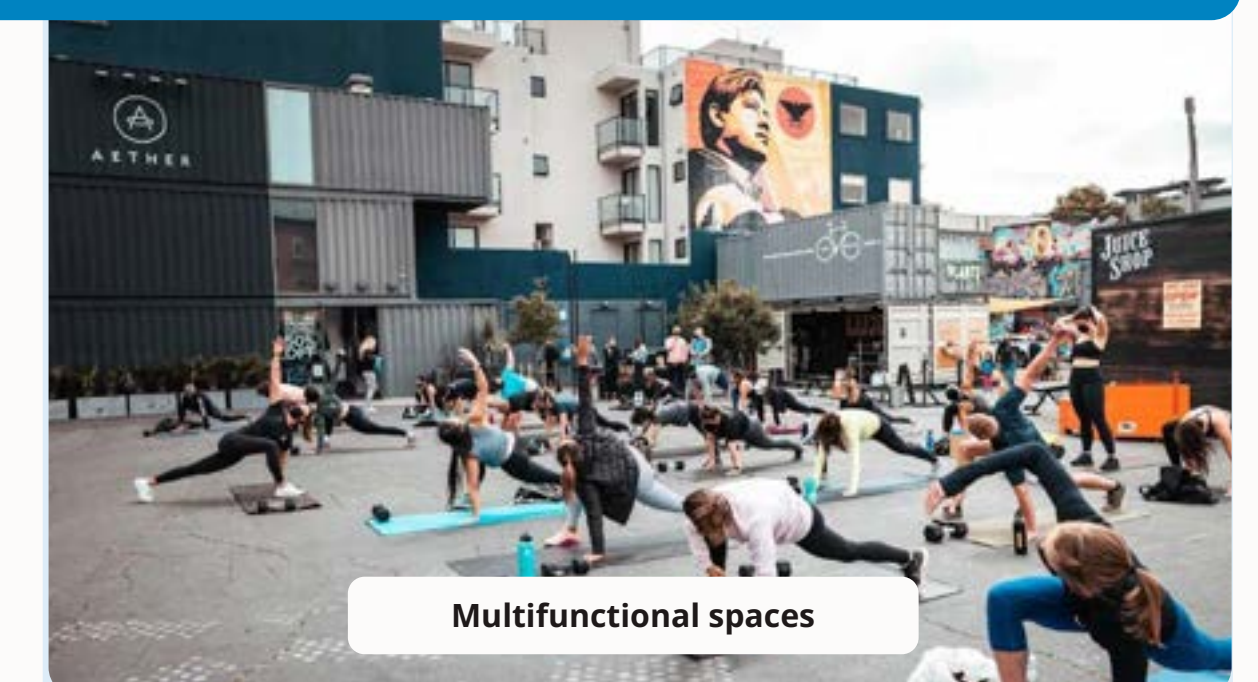
Public amenities in buildings



'Acupuncture' retail



Repurposed uses





TRANSPORTATION

Moving to, from, & around Central Lonsdale



Lonsdale Avenue: How does our street design impact our community?

Right now, Lonsdale Avenue is designed mostly for cars, with about 70% of the space dedicated to driving and parking, leaving just around 30% of the space for walking and rolling, street trees, furnishings, and other amenities. This spatial layout reflects how people currently travel in the area—around 60% of trips are by car while 20% are by walking¹. The way the space is designed influences how people move, as design shapes behavior.

This car-first street design impacts the experience of being on Lonsdale Avenue. Traffic brings noise and affects air quality, making the street less welcoming for pedestrians. When people don't feel comfortable walking or lingering, fewer choose to spend time in shops, restaurants and public spaces—ultimately affecting the street's vibrancy and economic vitality.

1. City of North Vancouver. (2021). 2021 North Shore Transportation Survey Report.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



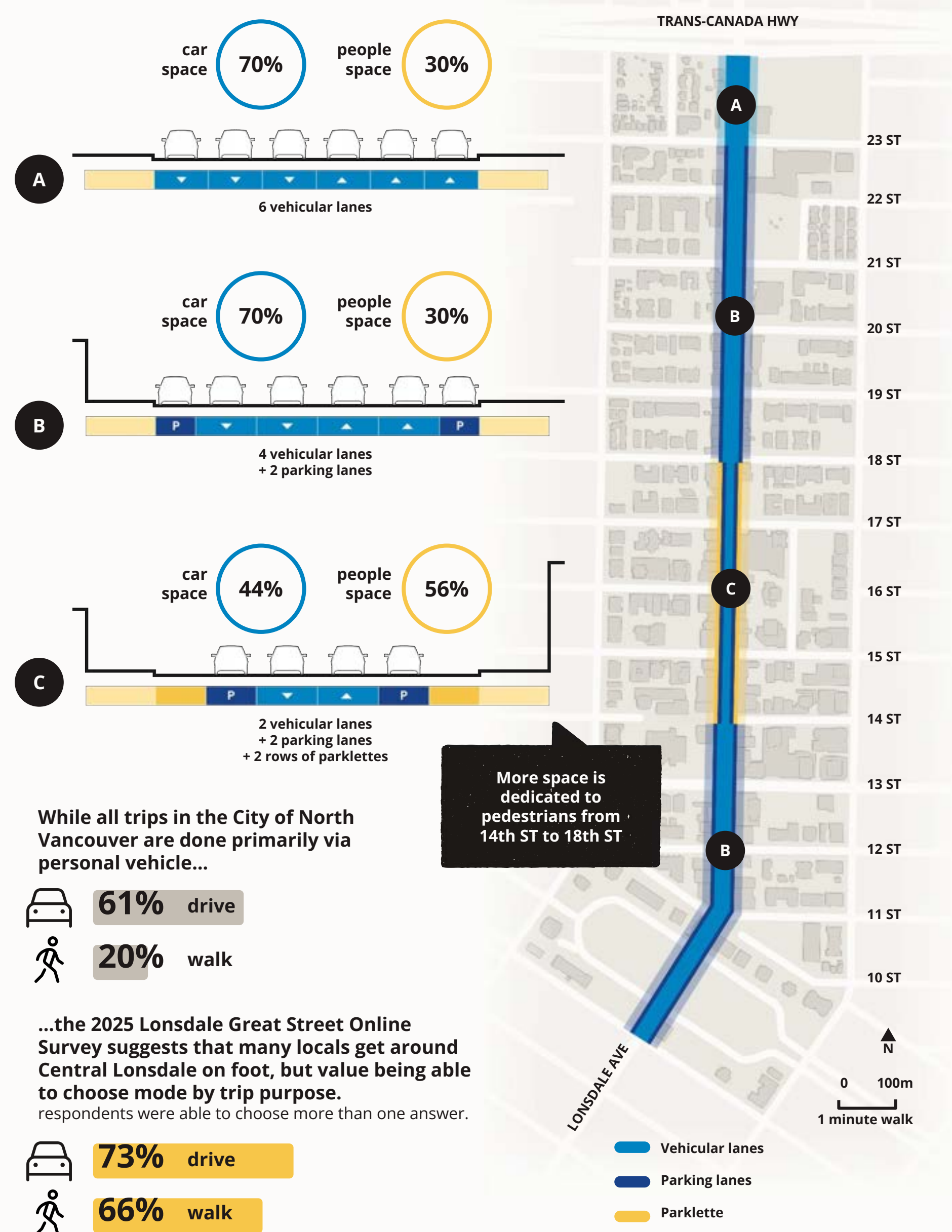
What we saw:



“Too many fast loud cars spoil an otherwise good experience. Calming traffic would be a big help.”
— 2025 Lonsdale Great Street Survey



“I would like to see Lonsdale Avenue restricted to 2 lanes for cars. 4 lanes is too busy, too loud from the traffic.”
— 2025 Lonsdale Great Street Survey

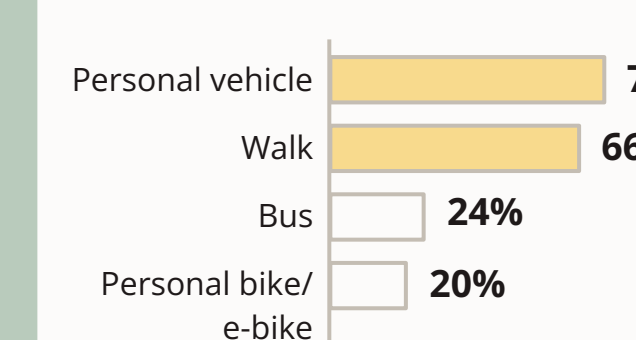


What we heard:

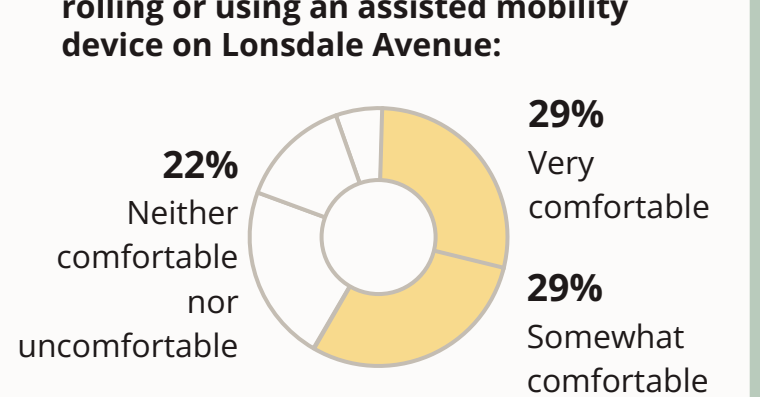
“Although walkable, it still feels like vehicles have the right of way in the area. It's very loud and sitting outside on sidewalk patios and benches is not enjoyable because of it.”
— 2025 Lonsdale Great Street Survey

“Pedestrian safety should be a priority.”
— 2025 Lonsdale Great Street Survey

How do you get to Lonsdale Avenue:



How comfortable do you feel walking, rolling or using an assisted mobility device on Lonsdale Avenue:



Opportunities:

- **Optimize the right-of-way:** Explore opportunities to reduce space dedicated to vehicle movement for enhanced public realm improving the pedestrian experience.
- **Extend public realm:** Explore opportunities to add bump-outs at intersections and reduce on-street parking in strategic locations to extend the public realm offerings. Temporary or seasonal opportunities may be explored.
- **Improve traffic calming:** Introduce slower traffic speeds and traffic calming measures to prioritize pedestrians and create a safer, more pedestrian friendly environment.
- **Safety measures:** Explore opportunities to support public safety through increased public realm, adequate buffer space between pedestrians and vehicles and clearly defined areas for micro-mobility.

Not important

Very important



Lonsdale Transit: How can we connect transit with other ways of getting around?

Lonsdale Avenue is a key transit corridor, served by four bus routes and one night route. However, bus reliability is often affected by traffic, congestion and cars double parking, leading to delays.

The experience of waiting for a bus also varies widely along the corridor. Some stops have shelters, seating and clear signage, while others offer little protection or transit information, making transit feel inconsistent and less-user friendly.

By improving service reliability and creating a more comfortable, cohesive experience for riders, transit can become a more appealing and dependable option—helping support growth and a thriving, connected neighbourhood.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:



What we heard:

Survey respondents reported on improvements that would make getting to, from, and around Central Lonsdale a better experience:

- 53% convenient parking
- 41% pedestrian friendly streets
- 25% improved bus transit
- 23% prioritizing ped and cyclist comfort over vehicle speed

“Please make it a people-centred space by deprioritizing personal vehicles and prioritizing walking, parklets, active transportation, public transit etc.”
— 2025 Lonsdale Great Street Survey

Opportunities:

- **Transit priority measures:** Investigate transit priority strategies, including transit priority signals and dedicated/non dedicated transit lanes to enhance service reliability throughout the corridor.
- **Upgraded stop amenities:** Identify improved stop infrastructure, ensuring alignment with future development plans and land use revitalization efforts.
- **Enhanced boarding:** Raised bus stop curbs at high traffic stops can be used to attain near-level boarding with buses allowing faster boarding and alighting times.
- **Enhance first mile/last mile integration:** Explore opportunities to integrate multi-modal infrastructure near bus stops, including bike racks and shared mobility options to support seamless connections between modes.

Not important

Very important



TRANSPORTATION

Moving to, from, & around Central Lonsdale

Lonsdale Avenue: How can we make cycling a more convenient and appealing option?

Cycling to and along Lonsdale Avenue faces several challenges. The absence of safe, continuous routes or bike lanes makes it difficult to integrate cycling into daily travel, discouraging many potential cyclists from using the Lonsdale corridor.

Chesterfield and St Andrews Avenues have been designated as the primary north-south cycling routes, but cyclists still face intersections with heavy traffic, poor visibility and limited cycling infrastructure making it difficult to safely cross or turn onto Lonsdale Avenue. Adding to these challenges, the steep terrain creates a major barrier for those traveling north from Lower Lonsdale, making the first stretch difficult for many riders, especially those less experienced.

While some bike parking and facilities exist near commercial areas, the overall experience for cyclists—both in reaching Lonsdale Avenue and navigating it—remains difficult due to gaps in infrastructure and a lack of support for cycling as a viable mode of transport.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

CNV modal split

- 61% drive
- 20% walk
- 17% transit
- 2% bike

2025 Lonsdale Great Street Survey modal split

- 73% drive
- 66% walk
- 24% transit
- 20% bike

What we heard:

“Bike lanes should be designed with all users in mind...”
— 2025 Lonsdale Great Street Survey

How comfortable or uncomfortable do you feel biking on Lonsdale Avenue?

- 25% Very uncomfortable
- 35% Somewhat comfortable
- 12% Very comfortable
- 19% Somewhat comfortable
- 8% Neither comfortable nor uncomfortable

61% of respondents don't feel comfortable biking on Lonsdale Ave

Opportunities:

- **Cycle specific wayfinding:** Specific wayfinding and cycling infrastructure will help encourage and promote safer and more journeys.
- **Intersection improvements:** Advanced stop lines, and cycling-specific signals will promote safe cycling practices.
- **Quality arrival experience:** Integrate safe dismount zones and quality bike parking near key destinations including covered and protected.
- **Micro-mobility hubs:** Encourage micro-mobility hubs in close proximity to transit, new developments and key destinations within the community.



Lonsdale Curbside: How can we balance the needs of residents, businesses, and deliveries?

The City of North Vancouver is currently undertaking a Curb Access and Parking Plan to help further support improved curbside management in the city.

As it is today, curbside regulations have not adequately adapted to the increasing residential and commercial demand along Lonsdale Avenue. This has resulted in challenges for accessing curb space which impacts deliveries, pick-up-drop-off, and the overall functionality and efficiency of the street.

Observed parking occupancy in high-demand areas tends to be at or near 100%, causing further disruptions to traffic flow and impacting the reliability of public transit. In contrast, lower-demand areas, such as the northern end of Lonsdale, see very limited use of curbside parking, highlighting an imbalance in how space is utilized along the corridor.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

What we heard:

53% a TOP most desired choice: respondents reported that getting to, from, and around Central Lonsdale would be a better experience with **convenient parking options**.

In response to “Which of the following improvements would make getting to, from, and around Central Lonsdale a better experience for you?” Other top statements were a more vibrant shopping and dining experience (45%) and more pedestrian friendly streets (41%).

“It would be great to find a solution that makes parking more convenient for people coming from a distance and decreases the congestion on Lonsdale...”
— 2025 Lonsdale Great Street Survey

“I wish it was a more pleasant experience for both pedestrians and car users.”
— 2025 Lonsdale Great Street Survey

Opportunities:

- **Dynamic curbside management:** The use of real-time data and innovative technologies (sensors, dynamic curb pricing) to optimize curb space can help manage the increasing demand for curb access. Leveraging data can dynamically allocated space to different uses throughout the day.
- **Microdelivery hubs:** Transfer points where goods are unloaded from larger delivery trucks and then distributed to their final destinations using smaller, more sustainable vehicles to streamline the last mile delivery process.
- **Expand pay parking:** Pay parking in high demand areas will help increase turnover better regulating supply.
- **Expand loading zones:** More frequently placed loading zones co-located in areas with high demand will improve the efficient and safe movement of people and goods.





UTILITIES & INFRASTRUCTURE

Servicing Central Lonsdale



Central Lonsdale: How do we ensure a safe and reliable water supply?

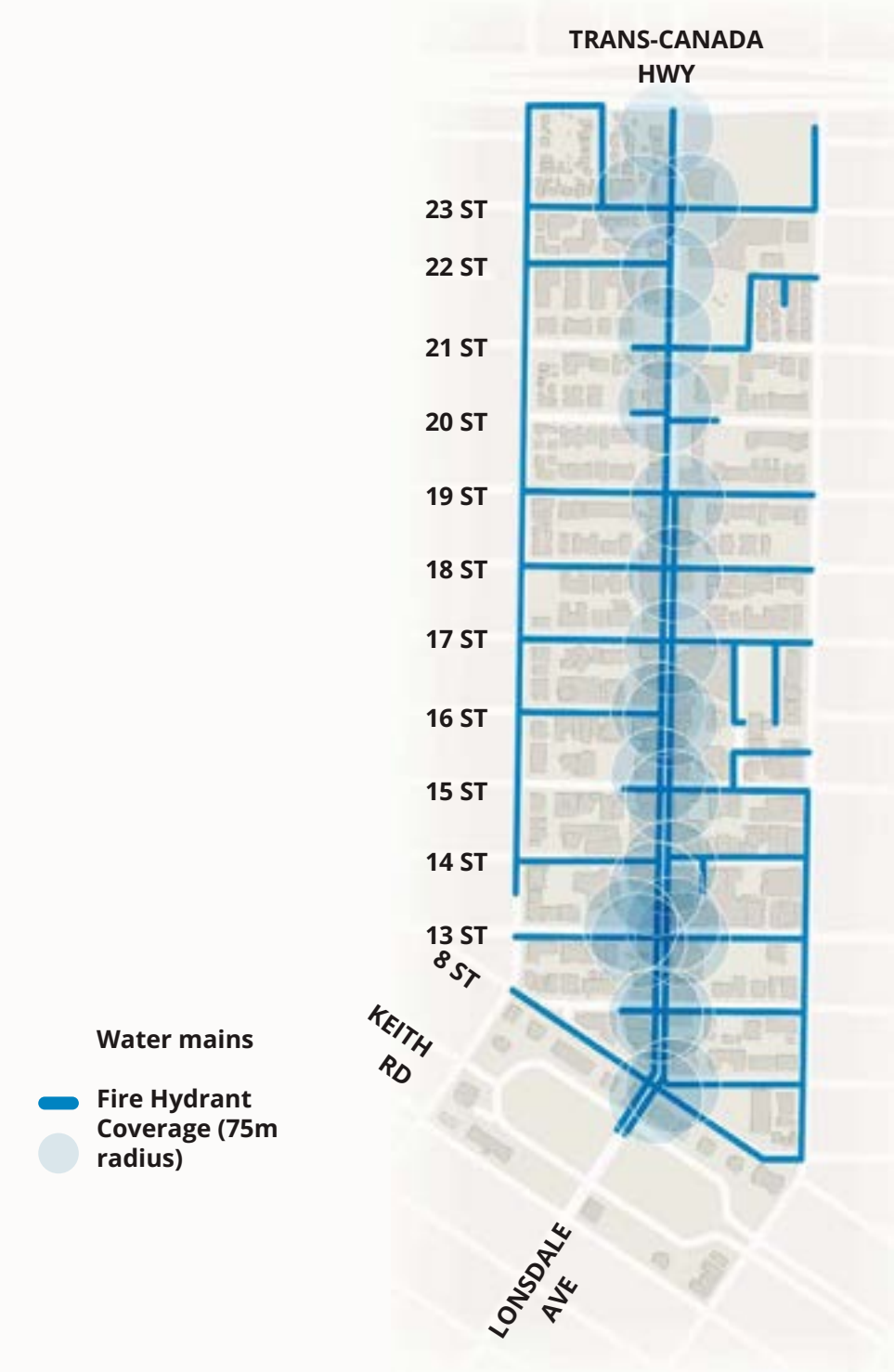
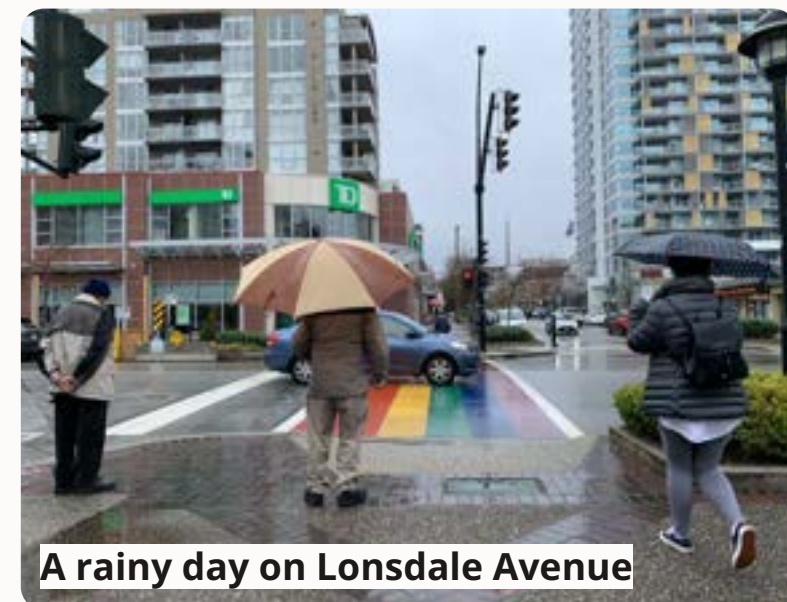
The water main infrastructure consists of pipes with installations dating back to 1930. The age of the existing water mains indicates a need for upgrades, as these pipes are susceptible to failures and inefficiencies. Disruptions due to pipe maintenance or failure will impact areas beyond the study location, as the central water mains along Lonsdale Avenue are directly serviced by Metro Vancouver Capilano Main No. 7, which provides water supply to the City. The existing hydrants appear to be in conformance with the City's hydrant spacing and location requirements, however, hydrant capacity and coverage may need to be reassessed to better support increased density in the area.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:



Not important

Very important



Central Lonsdale Sewer: How can we make our sanitary system more sustainable?

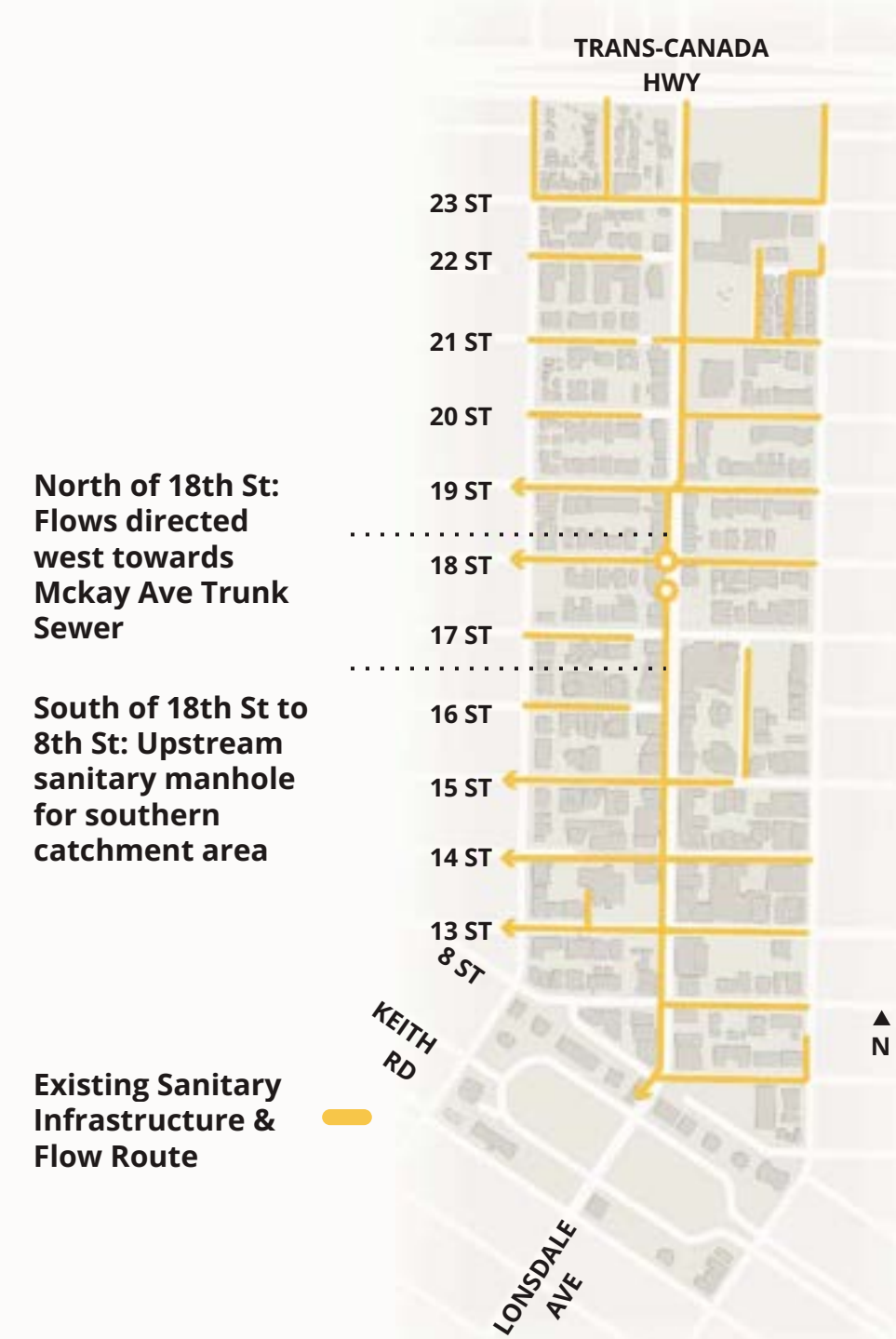
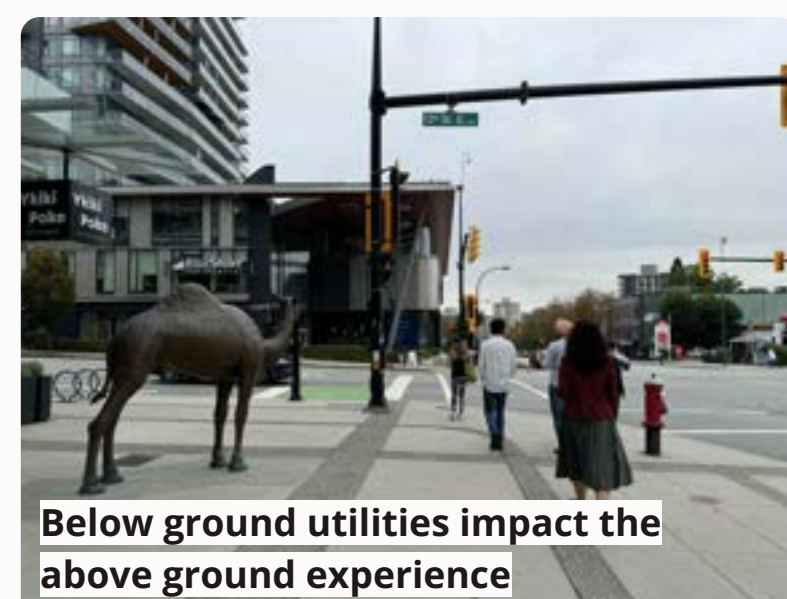
The gravity-fed sanitary sewer system in Lonsdale Avenue, divided into two catchments north and south of 18th Street, directs flows southwest to the Metro Vancouver Mackay Avenue Trunk Sewer, with pipes installed along specific alignments on the east and west sides of the roadway and vitrified clay pipes dating back to 1910-1911. Given the lack of CCTV inspection reports, aging infrastructure, and increased demands, a hydraulic capacity analysis is required to determine feasibility for continued use, and replacement of these utilities nearing the end of their service life should be considered.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:



Not important

Very important



Wagg Creek: How can we better manage stormwater runoff?

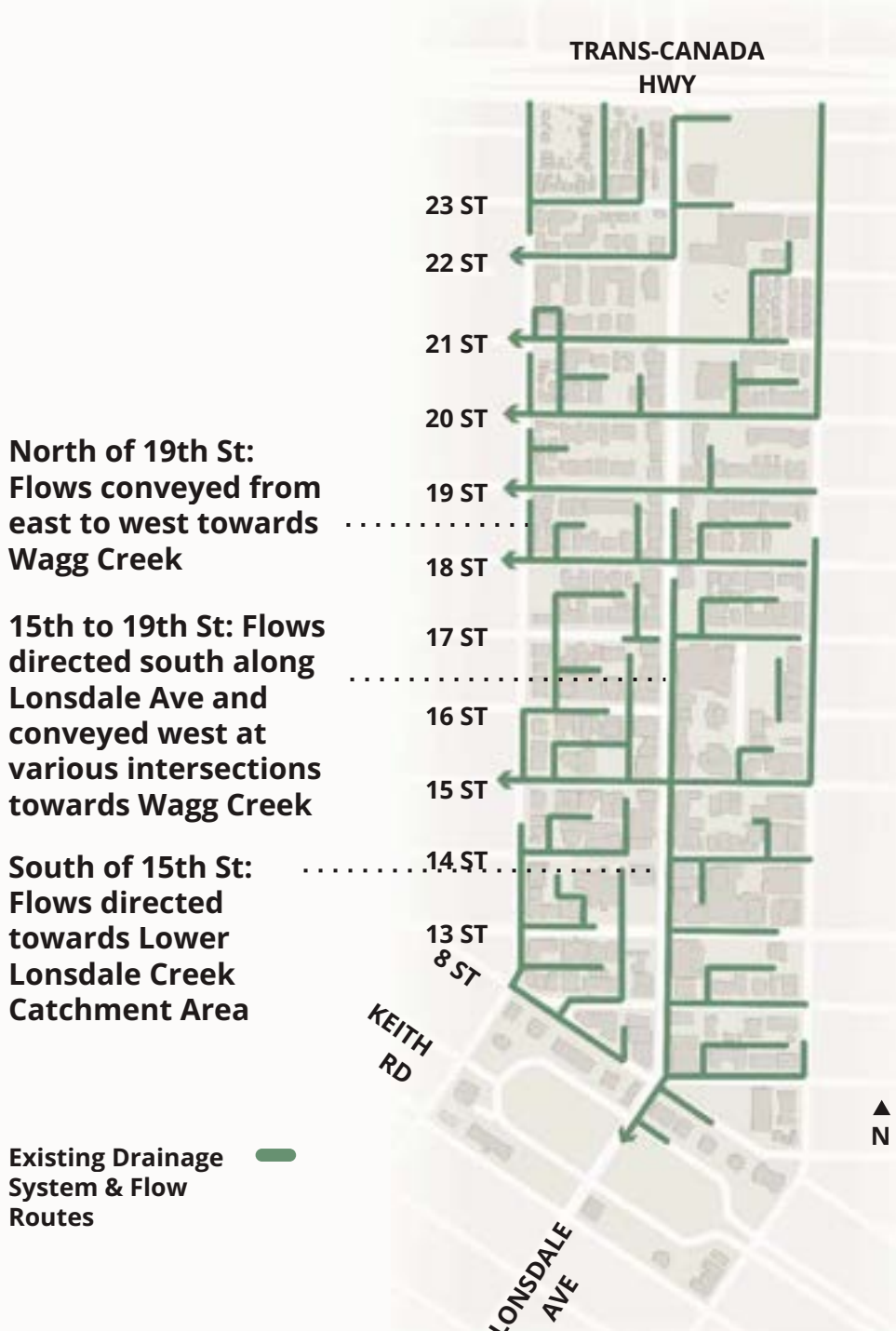
The City of North Vancouver uses separate systems for stormwater and sanitary sewage, and runoff from Central Lonsdale flows into the 4.4 km² Wagg Creek Watershed. Three main drainage patterns operate within the study area and a storm main ranging from 200-600mm diameter runs along Lonsdale Avenue from 19th Street to 8th Street. Though Wagg Creek is a fish bearing stream it is also the City's most urbanized with a total impervious area exceeding 50%. This, along with regular peak flows and water quality issues, has contributed to its degradation over the years. The City's Integrated Stormwater Management Plan reflects efforts to implement green stormwater source controls but there remains opportunity to expand on-site stormwater management features to mitigate runoff.

How important is this to you?

Place a sticker on the slider and add notes to a post-it to let us know what you think.



What we saw:



Not important

Very important

Opportunities:

- **Prioritize water main replacement:** Develop a phased plan for replacing aging water mains, prioritizing those in critical condition or with a history of failures.
- **Explore trenchless technologies:** Utilize trenchless technologies, such as pipe relining, to minimize disruption to businesses and residents during upgrades.
- **Enhance leak detection:** Implement advanced leak detection technologies to identify and repair leaks quickly, reducing water loss and preventing costly damage.
- **Conduct comprehensive condition assessments, including CCTV inspections of watermains system.**

Opportunities:

- **Assess system capacity:** Conduct a comprehensive assessment of the sanitary sewer system's capacity, including CCTV inspections of sanitary system, to identify potential bottlenecks and ensure it can accommodate future growth.
- **Upgrade aging infrastructure:** Prioritize the replacement of aging sewer lines, particularly those with a high risk of failure or capacity limitations.
- **Implement green infrastructure:** Incorporate green infrastructure, such as rain gardens and bioswales, to reduce the burden on the sanitary sewer system and improve water quality.

Opportunities:

- **Review hydraulic capacity:** Assess feasibility of the continued use of existing infrastructure through a review of hydraulic capacity.
- **Expand green infrastructure:** Integrate green infrastructure, such as Silva Cells, rain gardens, and green roofs, to capture and treat rainwater on site, reducing runoff, improving water quality and relieving pressure on Wagg Creek.
- **Enhance existing rain gardens:** Improve the performance and aesthetic appeal of existing rain gardens through regular maintenance, targeted plant selection, and design enhancements to augment the public realm in addition to reducing runoff.
- **Utilize pervious surfaces:** Incorporate permeable paving materials in sidewalks, plazas, and parking areas to allow rainwater to infiltrate into the ground, replenishing groundwater supplies.
- **Mitigate construction impacts:** Implement erosion control measures, sediment basins, and other best practices during construction to minimize the impact on Wagg Creek and surrounding waterways.



UTILITIES & INFRASTRUCTURE

Servicing Central Lonsdale

Lonsdale Energy: How can we incorporate even more renewable energy?

Since 2003, the Lonsdale Energy Corporation has delivered renewable and efficient energy to Central Lonsdale and nearby areas through an underground network of pipes and mini-plants. Key features include solar panels on the City Library and a ground source heat pump beneath the School District office, enhancing energy efficiency and reducing emissions.

Already interconnected with Harbourside and Lower Lonsdale, the system is expanding under the Vancouver Hydronic Heat Energy Service Bylaw requiring new buildings over 1,000 m² to be connected to the district energy system. Further discussions with the City will shape the LEC's role in the revitalized corridor.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

Central Lonsdale - Photo from Clearway Real Estate

What we heard:

“I hope that any upgrades have a minimum impact on environmental degradation and are consistent with goals for achieving a net zero community by 2050 and being resilient to anticipated changes in climate for our region...”

- 2025 Lonsdale Great Street Survey

Opportunities:

- **Expand the LEC network:** Extend the LEC network to serve new developments and encourage the connection of existing buildings, maximizing the benefits of clean, district energy.
- **Promote renewable energy sources:** Further incorporate renewable energy sources, such as solar panels and geothermal heat pumps, into the LEC system to further reduce its carbon footprint.
- **Educate the community:** Raise awareness of the benefits of district energy and encourage community participation in the LEC program.



Central Lonsdale Infrastructure: Can infrastructure upgrades help us grow our urban forest?

Designing the subsurface infrastructure together with the public realm enhancements offers a chance to relocate and consolidate underground infrastructure, freeing up valuable space above ground to enhance the public realm. Strategic utility consolidation can create more room for green infrastructure, such as expanded tree canopies, native plantings, and efficient rainwater management.

This investment can boost ecological resilience while delivering strong economic returns by lowering maintenance costs, reducing urban heat effects, and improving stormwater management—cutting expenses tied to flooding and drainage repairs. It's about making the most of the space beneath our feet to create a more livable, thriving city above ground.

How important is this to you?
Place a sticker on the slider and add notes to a post-it to let us know what you think.

What we saw:

Mid-construction, biofiltration garden

Biofiltration garden with permeable surface area and pollinator habitats

Integrated rain gardens within curb extensions at intersections

Right-of-Way: 30.0m
Curb-to-Curb: 21.0m

Location of Utilities Typical Section Lonsdale Ave and 18th St

- WAT (250mm)
- SAN (200mm)
- LEC DES
- STM (200mm)

Opportunities

- **Combine underground utilities:** Water pipes, sewer lines, and other underground systems can be combined in a common trench to free up space for improvements above ground.
- **Link investments:** Soil cells can be integrated with the stormwater management systems, allowing water to be directed into tree pits. This helps retain water within the system, providing benefits to the trees while also reducing strain on municipal stormwater infrastructure
- **Make room for more green spaces:** Use space savings from consolidated infrastructure replacement to plant more trees, create rain gardens, and add other green features that make the streets more environmentally friendly.
- **Coordinate construction projects:** Combine pipe replacement and utility upgrades with other road work or building projects to minimize disruptions.
- **Future proof development:** Upgrades to municipal infrastructure in alignment with rezoning can make redevelopment more attractive to the private sector spurring new development opportunities in the community.





TRANSPORTATION, UTILITIES & INFRASTRUCTURE

Inspiration Images

Mode balance



Multimodal street



Pedestrian-priority



People-first streets

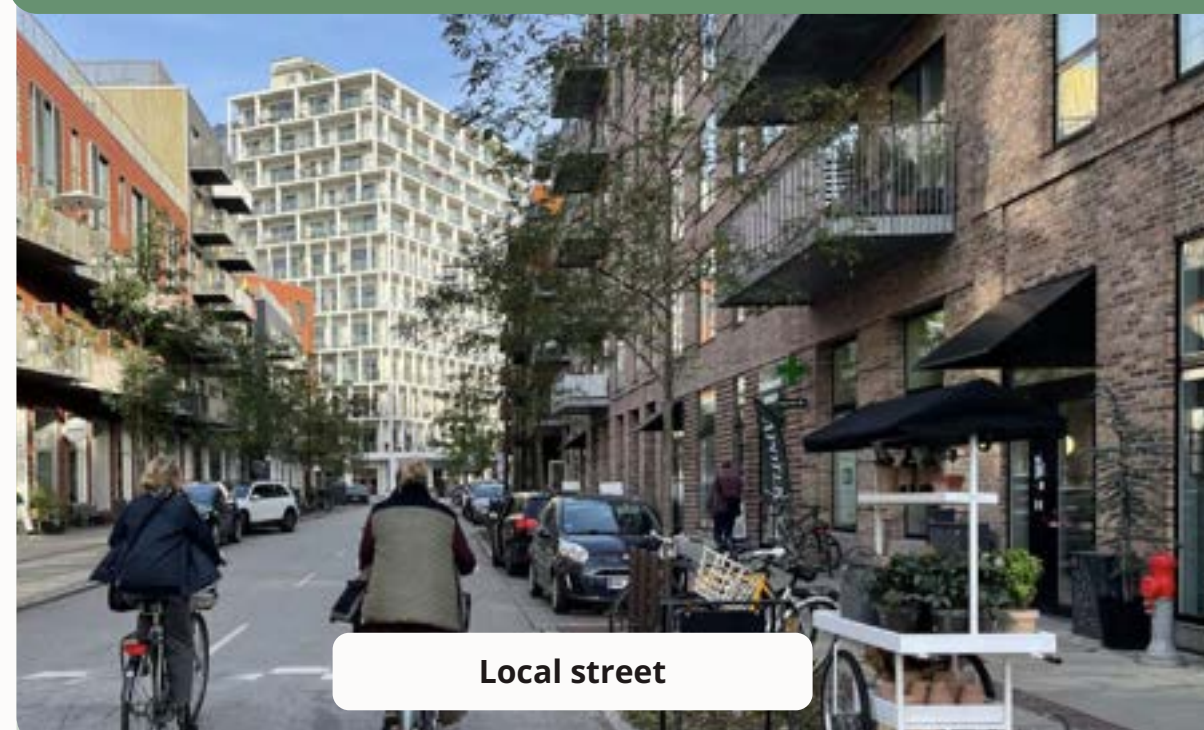


Diverse modes



Designated travel lanes

Street network



Local street



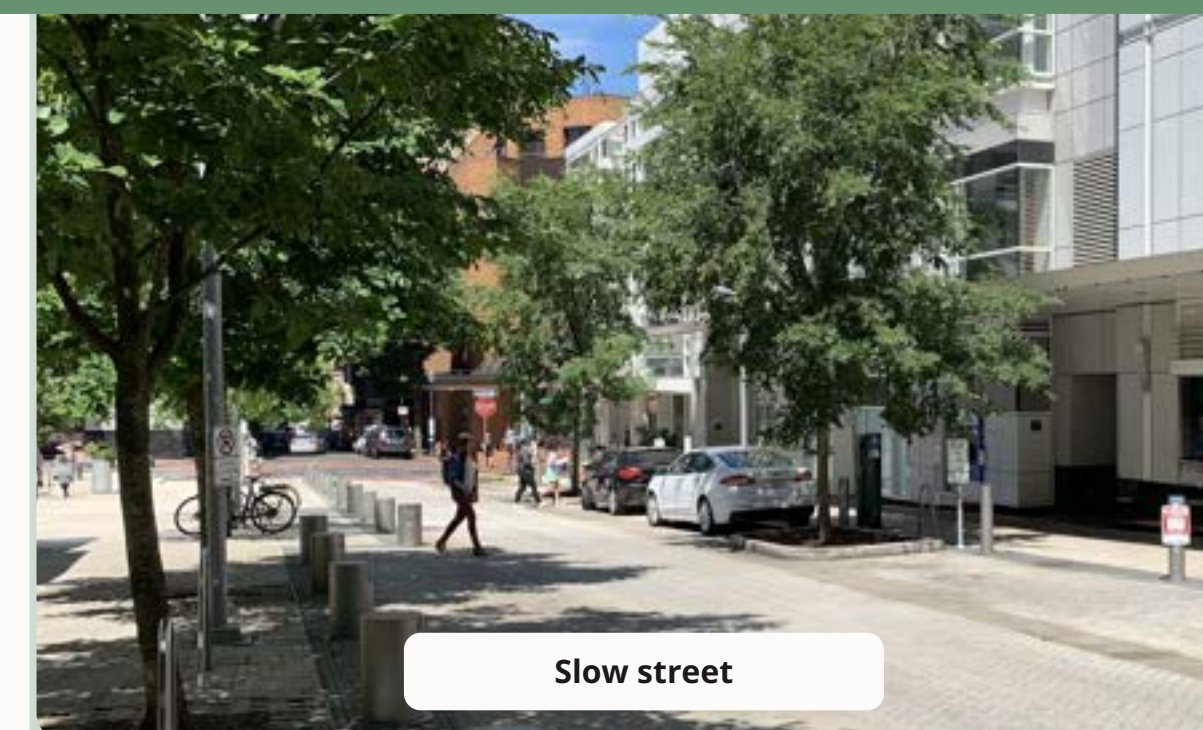
Connector street



Biodiverse/green corridor



Transit street



Slow street

Transit experience



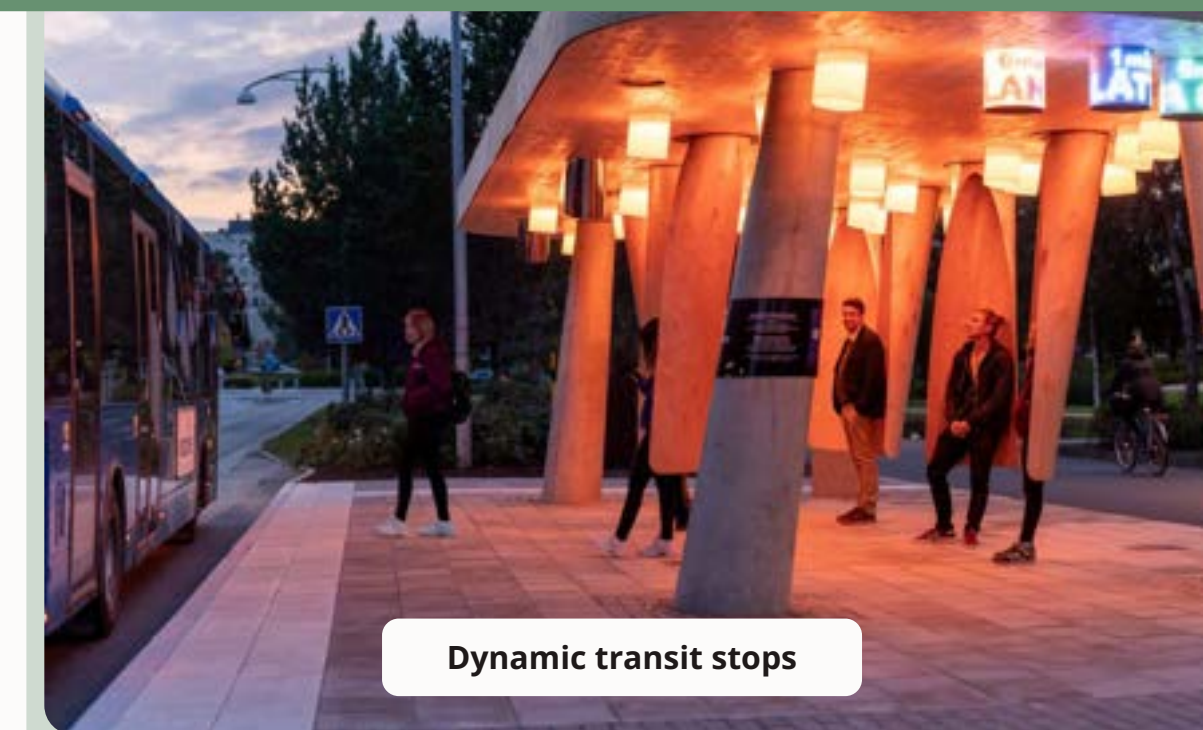
Seamless transit experience



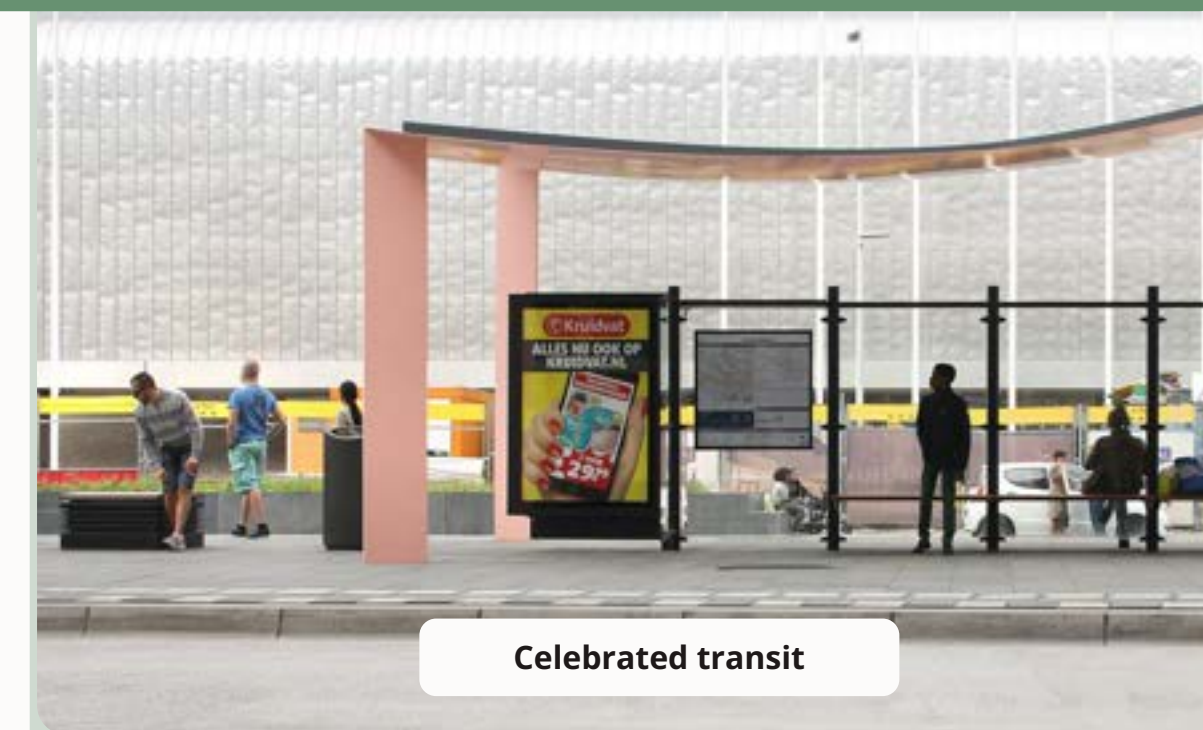
Transit stops with amenities



Accessible and dignified waiting areas



Dynamic transit stops



Celebrated transit

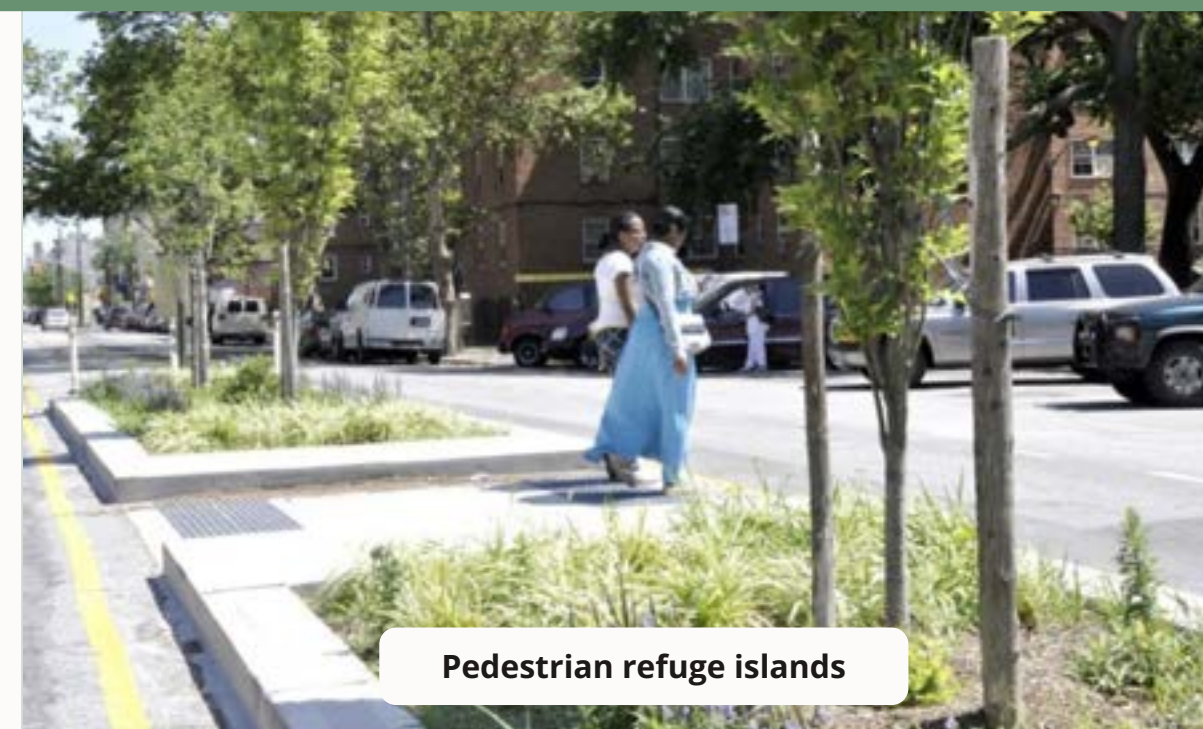
Intersection treatment



Accessible crosswalks



Reduced crossing distance



Pedestrian refuge islands

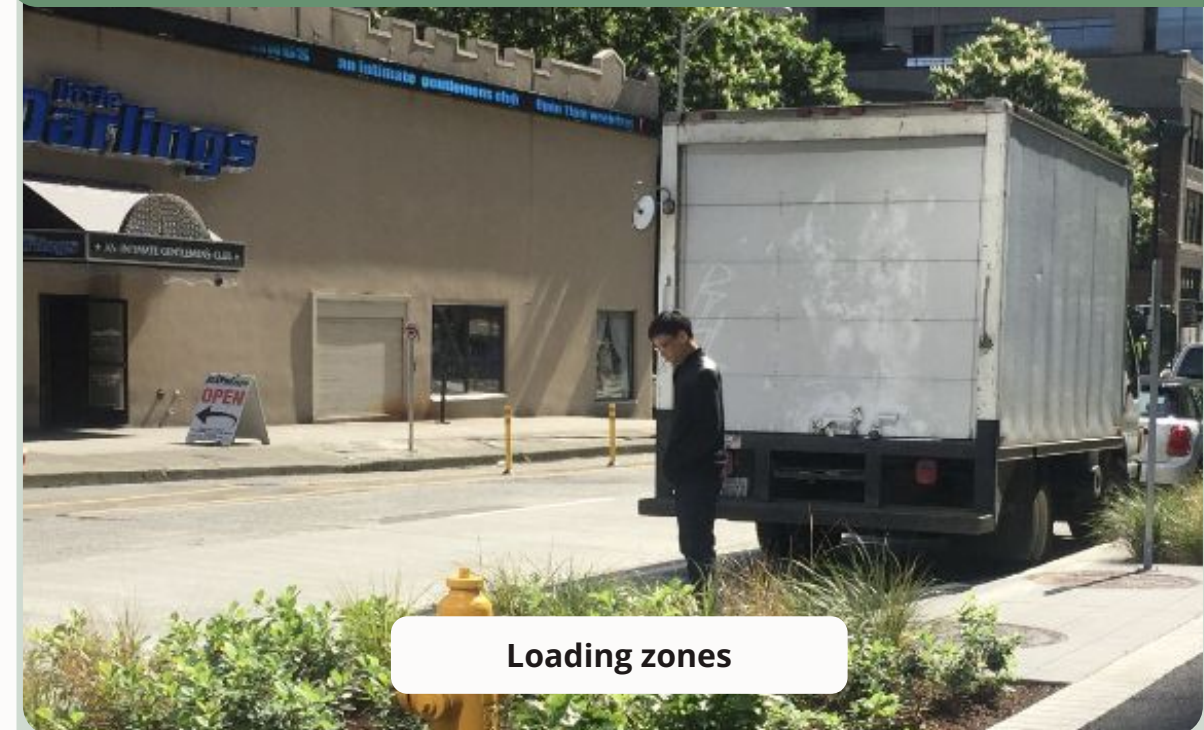


Flush intersections



Pedestrian-friendly intersections

Curbside management



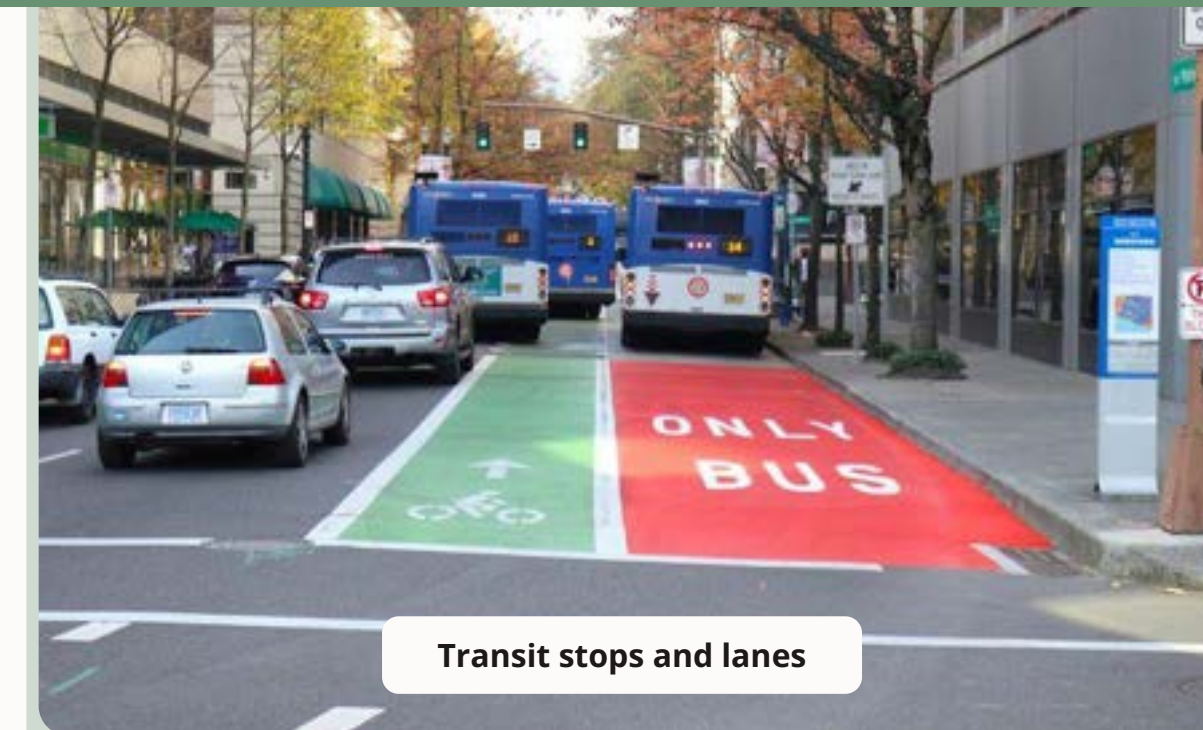
Loading zones



Parking zones



Pick up-drop off zones



Transit stops and lanes

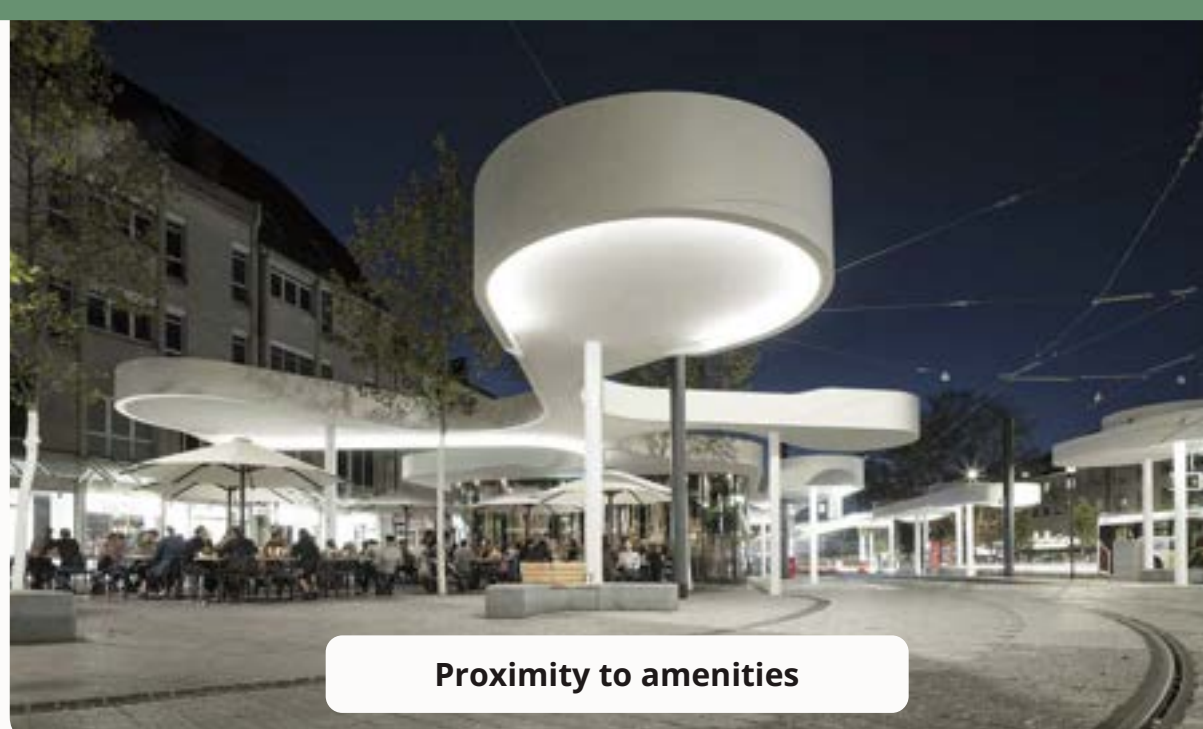


Sidewalk extensions for traffic calming and greenery

Arrival experience and mode transfer



Arrival hubs



Proximity to amenities



Seamless mode transfer



Covered bike parking



Designated micro mobility parking



TRANSPORTATION, UTILITIES & INFRASTRUCTURE

Tell us your thoughts & ideas!

Q

What would make Lonsdale Avenue and Central Lonsdale feel safer and more comfortable for pedestrians?

Q

What types of improvements are important to encourage more people to choose active mobility (e.g., biking, walking) and public transit in Central Lonsdale?

Q

Is there anything else you want on this topic?





FEEDBACK

What did we miss?

Use sticky notes
to tell us what
you think about
the future of
Lonsdale Avenue: